



Liverpoolcitycouncil
creating our future together

Our Ref: 246518.2013
Contact: Megan Munari 9821 9285

23 December 2013



PCU50528

Mr Peter Goth
The Department of Planning
GPO Box 39
SYDNEY NSW 2001

Dear Mr Goth

**Re: Planning Proposal for Liverpool Local Environmental Plan 2008
(Amendment No. 37)**

I am writing to advise that at its meeting held on 25 September 2013, Council resolved to amend the Liverpool Local Environmental Plan (LLEP) 2008. Draft LLEP 2008 Amendment No.37 seeks to amend Schedule 5 of the LLEP 2008 to include the Defence National Storage and Distribution Centre on the Local Heritage List.

At the meeting Council made the following decision:

That Council:

- 1. Forwards the Planning Proposal to add the Defence National Storage and Distribution Centre to the Liverpool Local Environmental Plan 2008 as a heritage item to the Department of Planning and Infrastructure for a Gateway determination.*
- 2. Writes to the Office of Environment and Heritage and encourage them to list the site on the State Heritage Register.*

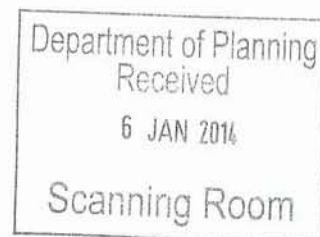
A copy of the planning proposal in accordance with "A guide to preparing planning proposals", the aforementioned report to Council and other documentation on this matter is enclosed for your consideration.

In accordance with Section 56 of the *Environmental Planning and Assessment Act 1979*, Council requests a Gateway Determination regarding the attached Planning Proposal.

Liverpool City Council looks forward to working with the Department of Planning to progress this proposal. If you would like to discuss any aspect of the attached documentation please contact Megan Munari, Senior Strategic Planner on 9821 9285, or by email m.munari@liverpool.nsw.gov.au.

Yours sincerely

Tanya O'Brien
Manager Strategic Planning



Enc.

Customer Service Centre Level 2, 33 Moore Street, Liverpool NSW 2170, DX 5030 Liverpool

All correspondence to The General Manager, Locked Bag 7064 Liverpool BC NSW 1871 **Call Centre** 1300 36 2170
Fax 9821 9333 **Email** lcc@liverpool.nsw.gov.au **Web** www.liverpool.nsw.gov.au **NRS** 13 36 77 **ABN** 84 181 182 471

State Heritage Register Nomination Form

Important notes for nominators

Before completing this form, read the *Guideline for Nominations to the State Heritage Register** to check whether a nomination is appropriate.

You should only nominate an item you believe to be particularly important in the context of all of NSW, beyond the local area or region.

This is because only items of *state* heritage significance are eligible for listing on the State Heritage Register.

When completing this form note that:

- It must be completed and submitted electronically.*
- Follow the guideline* to fill out the form accurately.
- **YELLOW sections** of this form are mandatory for your nomination to be accepted for consideration.
- **BLUE sections** of this form are recommended, but are not essential, unless otherwise indicated in the form.
- The completed form must show *how* the nominated place is *state* significant to meet the criteria for listing on the register.
- Incomplete nominations, or those with insufficient information, may not be accepted.
- A complete and accurate nomination form, with a clear assessment of state significance of the place or object, will assist in the timely consideration of your nomination.

* Download this form and guideline at: www.heritage.nsw.gov.au/nominating

A. Nominated place

1. Name

Name of place / object:	Defence National Storage and Distribution Centre
Other or former name(s):	Mobilisation Stores

2. Location

Street address*:	Lot 1 (no.)	Moorebank Avenue (street name)	
	Moorebank (suburb / town)		
Alternate street address:	(no.)	(street name)	
	(suburb / town)		
Local government area:	Liverpool City Council		
Land parcel(s)*:	1 (Lot no/s)	(section no.)	1048263 (DP no.)
	(Lot no/s)	(section no.)	(DP no.)
	(Lot no/s)	(section no.)	(DP no.)
Co-ordinates*:	(Latitude)	(Longitude)	(Datum)

*At least one of these three location details must be provided. For a movable object, enter its principal location. If the place has no street number, provide land parcels. If it has no land parcels, provide Co-ordinates and a map.

3. Extent of nomination

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Curtilage map of nominated area:



Source of map or plan:	Liverpool City Council EView (aerial photography from NearMap August 2013)
Boundary description (in words):	The DNSDC site is a rectangular block of land covering approximately 108 hectares. The site is bounded by Anzac Road on the north, Moorebank Avenue on the west, the Greenhills Ave road reserve to the east, and an area of natural bushland on the south and east. The main entrance to the site is located midway along Moorebank Avenue.

4. Ownership

Name of owner(s):	Trust Company Limited c/- Goodman Property Services (Aust) Pty Ltd	
Contact person:		
Contact position:		
Postal address:	GPO Box 4703 (street address or postal box)	
	Sydney (suburb or town)	
	NSW (state)	2001 (post code)
Phone number:		
Ownership explanation:		

B. Significance

State Heritage Register Nomination Form

5. Why is it important in NSW?

Statement of state significance:	The Defence National Storage and Distribution Centre (DNSDC) is of State significance for its historical, associational, aesthetic and social significance. It is also rare and representative. As a military storage site it dates from 1915 and is important for its associations with the development of Australia's military forces prior to and during World War I and particularly for its direct association with the military build up in the early years of World War II. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The DNSDC contains 20 World War II post and beam warehouses, many of which, despite being re-clad, are good examples of their type. Particularly important are the 15 timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Also important are the three composite steel and timber type warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value. The DNSDC is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site. The place also has an association with early nineteenth century settlement in the Liverpool area.
Comparisons:	Post and beam stores buildings were built during World War II in various military establishments in eastern Australia, and particularly in south-eastern Australia. They played a major military warehousing role at the time. In 1995 a number remained in use, including at Bogan Gate, Regents Park and Wallangarra in NSW and at Broadmeadows and Bandiana in Victoria. However, these types of buildings are now rare. By 2002 Bandiana was the only other known site with similar World War II timber store buildings still in Defence ownership. Hence the examples at DNSDC have rarity value as well as being good examples of the type (particularly in the case of the nine-bay types).

C. Description

6. Existing place or object

Description:	The site includes a number of large storage sheds along with smaller ancillary, administration, and workshop buildings. Among these structures are 20 timber post and beam buildings dating to World War II. Fifteen of these are of timber post and beam construction, with nine internal bays. They retain their original timber structure, though
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	they have been reclad with modern steel sheeting, and have new concrete floors. Three of the buildings are composite timber and steel warehouses which have three bays of timber post and beam construction on either side of a central raised bay. The central bay has a steel frame to support an overhead gantry crane. The final two buildings are the smaller Quartermaster's Store, with five bays of timber post and beam construction, and the Carpentry Workshop, which are timber framed and three bays wide. Both the Quartermaster's Store and the Carpentry Workshop are constructed of Oregon, an American wood. The alignments of rail sidings that once ran through the site are still visible in the landscape through the location and orientation of some of the buildings and roads, while to the south a remaining siding is still clearly visible. A number of buildings were constructed within the DNSDC site in the mid-late 1990s. These include a cluster of buildings in the south western corner of the site and a number of buildings in the northern section of the site.
Condition of fabric and/or archaeological potential:	Understood to be in good condition. The timber post and beam buildings at DNSDC were reclad in the early 1990s.
Integrity / intactness:	Good: The timber post and beam buildings at DNSDC were reclad in the early 1990s, removing the original asbestos cladding. A small number of buildings were also removed at this time.
Modifications Dates:	The original asbestos cladding of the timber post and beam buildings at DNSDC was replaced in the early 1990s. New structures were also added to the complex in the 1990s. The DNSDC was sold into private ownership c.2003.
Date you inspected the place for this description:	Defence land. Inspection not possible.
Current use*:	Defence usage continues via lease from the new private owner.
Original or former use(s)*:	Military storage and distribution.
Further comments:	The place is currently listed on the Commonwealth Heritage List (CHL) however once the lease ends and the military use is removed, the CHL listing will be defunct. This valuable heritage place needs ongoing protection to enable appropriate management and ensure its conservation in the future. Liverpool City Council is in the process of including the DNSDC in the Environmental heritage list (Schedule 5) of the Liverpool Local Environmental Plan 2008.

Illustrations can be inserted as images in section J.

* These details must be entered if you are basing the significance of the place or object on its past or present use.

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D. Historical outline

7. Origins and historical evolution

Years of construction*:	1915 (start year)	1990s (finish year)
Designer / architect*:		
Maker/ builder:		
Historical outline of place or object:	The Defence National Storage and Distribution Centre (DNSDC) at Moorebank, which is part of the larger Moorebank Defence site, stands within part of the original 6000 acre property granted to Thomas Moore in 1805-21. The eastern boundary of the DNSDC site follows the alignment of part of Moore's property. Moore was the first magistrate at Liverpool and was an influential figure in the early years of the area. The northern and western boundaries of the DNSDC follow boundaries of the later 1888 Moorebank Farms subdivision. Meanwhile, military activities began in the Liverpool area early in the 1800s when troops guarded convicts and a military barracks was constructed in the town. Military use of the area increased early in the twentieth century, and training camps were run. Military manoeuvres were held for the inspection of Lord Kitchener in 1910, and a rifle range was developed. The military camp was located partly on the present DNSDC site. One result of Kitchener's visit was the acquisition by the Commonwealth of large areas of land for military purposes, and in 1913 an area exceeding 16,000 acres and including the DNSDC site was acquired by the federal government for defence purposes. This was the era of compulsory training and permanent military training areas, hence the Commonwealth's need to acquire large areas. Military storage at the DNSDC appears to have begun in 1915. The Liverpool camp was the main military training centre in NSW during World War I. All new recruits came here, including Light Horse units. In 1919 two railway sidings were added in order to service the depot, and were part of the larger railway system which linked the various military establishments in the area. One of the sidings was extended during World War II. As mentioned above, military storage began at what is now called the DNSDC in 1915, during World War I. Called initially the Mobilisation Stores, the place accommodated the 2nd Base Ordnance Depot and 2nd Base Workshops during World War II. In 1972 these units became the 21st Supply Battalion and the 2nd Base Workshop Battalion. In 1990 these two joined to become the Moorebank Logistics Group. During World War II there were perhaps 20 timber post and beam	

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	buildings erected, as well as other buildings. All these structures reflected the widespread expansion of support facilities during the early part of the war. From the 1940s to the early 1990s there was little development at the site. Then in the early 1990s the place was upgraded to become the DNSDC. Most of the World War II buildings were reclad and the original asbestos cement sheeting was removed. A small number of buildings were demolished and replaced with more modern designs. The creation of the DNSDC was an element of the Defence Logistics Redevelopment Project which aimed at rationalising Defence warehousing. DNSDC is the main warehouse for all three of the Australian armed services, and there are maintenance sections as well. The timber used in the 20 post and beam buildings is Oregon. It is understood that the buildings were prefabricated in the United States and shipped to Australia on US Liberty Ships. Post and beam stores buildings were built during World War II in various military establishments in eastern Australia, and particularly in south-eastern Australia. They played a major military warehousing role at the time. In 1995 a number remained in use, including at Bogan Gate, Regents Park and Wallangarra in NSW and at Broadmeadows and Bandiana Victoria. However, these types of buildings are now fairly rare. By 2002 Bandiana was the only other known site with similar World War II timber store buildings still in Defence ownership. Hence the examples at DNSDC have rarity value as well as being good examples of the type (particularly in the case of the nine-bay types).
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* These details must be entered (as shown in the guideline) if you are basing the significance of the place or object on its architectural or technical qualities or its historical period of construction.

Historical maps, photographs or other illustrations that help to explain the history of the place can be inserted in section J.

8. Historical themes represented

National theme(s): <i>Select one or more of 1-9.</i> <i>New line for each selected.</i>	3. Developing local, regional, national economies 7. Governing 8. Developing Australia's cultural life
State theme(s): <i>Select one or more for each above selected National theme 1-9.</i> <i>New line for each selected.</i>	Within National themes 1-3: 3. Transport Within National themes 4-9: 7. Defence 8. Creative endeavour

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E. Criteria

9. Assessment under Heritage Council criteria of state significance

A. It is important in the course or pattern of the cultural or natural history of NSW.	It meets this criterion of State significance because of its association with the development of Australia's military forces since the early 20th century and particularly for its direct association with the military expansion in the early years of World War II. The site has played a continual role in Australia's military infrastructure until the present day.
B. It has a strong or special association with the life or works of a person, or group of persons of importance in the cultural or natural history of NSW.	It meets this criterion of State significance because it has very strong associations with the Australian Defence Forces.
C. It is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW.	It meets this criterion of State significance because the WWII buildings demonstrate the unique aesthetic characteristics of military buildings constructed during World War II, and a high degree of technical achievement. The DNSDC contains 20 World War II post and beam warehouses, many of which, despite being re-clad, are good examples of their type. In addition to the 15 timber post and beam military warehouses of the nine-bay type are the three composite steel and timber type warehouses, which are also important. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s.
D. It has strong or special association with a particular community or cultural group in NSW for social, cultural or spiritual reasons.	It meets this criterion of State significance because the site has social significance for the extensive community of Defence personnel who have worked at the site through its history, and for the local community of Liverpool and the broader community of Sydney, as the location of Defence operations since 1915.
E. It has potential to yield information that will contribute to an understanding of the cultural or natural history of NSW.	The place has moderate scientific significance for its ability to show evidence of the boundaries and alignments of the original land grants in the area, the 1888 Moorebank Farms subdivision, and part of the Liverpool – Anzac Rifle Range – Holsworthy military railway line. Low-moderate archaeological significance for its potential to

State Heritage Register Nomination Form

	yield information regarding the early use of the site and, particularly, its use during WWII. The extant WWII buildings have significance for their innovative construction method, as rare and representative examples of timber post and beam store buildings constructed during World War II.
F. It possesses uncommon, rare or endangered aspects of the cultural or natural history of NSW.	It meets this criterion of State significance because the group of World War II buildings at the site are the only known surviving group of such buildings in NSW in Defence use. The only other known site with similar World War II timber store buildings, and which remains in Defence ownership, is Bandiana, Victoria. Particularly important are the 15 timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value.
G. It is important in demonstrating the principal characteristics of a class of cultural or natural places/environments in NSW.	It meets this criterion of State significance because the timber post and beam store buildings are representative examples of this type of building constructed during World War II for military storage purposes throughout the east coast of Australia.

F. Listings

10. Existing heritage listings

Check one box for each of the following listings:

Listed	Not listed	
☐	☒	Local environmental plan (LEP) - heritage item (call the local council to confirm)
☐	☒	Regional environmental plan (REP) - heritage item (call the local council to confirm)
☐	☒	LEP - Conservation area (call the local council to confirm)
☒	☐	Draft LEP - Draft heritage item (call the local council to confirm)
☐	☒	Draft LEP - Draft conservation area (call the local council to confirm)
☐	☒	State Heritage Register (search the register at www.heritage.nsw.gov.au)
☐	☒	National Trust register (call the National Trust to confirm)
☐	☒	Aboriginal heritage information management system (by Dept. of Environment & Conservation)
☐	☒	Royal Australian Institute of Architects Register of 20 th Century Architecture (call the RAI A)
☐	☒	National shipwreck database (search maritime.heritage.nsw.gov.au/public/welcome.cfm)
☐	☒	Engineers Australia list (call Engineers Australia to confirm)
☐	☒	National Heritage List (search Australian Heritage Database www.deh.gov.au/cgi-bin/ahdb/search.pl)
☒	☐	Commonwealth Heritage List (search above Australian Heritage Database)
☒	☐	Register of the National Estate (search above Australian Heritage Database)

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☐	☒	NSW agency heritage and conservation section 170 register (call owner or occupier Agency)
Other listings:		

G. Photograph

11. Image 1

Principal photograph of place or object:	
Subject of photo:	Depiction of the DNSDC surrounded by the local heritage places the Holsworthy Group and Cubbitch Barta National Estate (shaded), showing the heritage protection of similar military heritage places at Moorebank with the stark omission of the DNSDC.
Date of photo:	August 2013
Photographer:	Nearmap
Copyright holder:	Nearmap

Include additional photographs and images in section J.

If possible, please also provide a high resolution, publication-quality copy of this image saved to disk (or as non-digital prints) with the signed hard-copy of the nomination form.

H. Author

12. Primary author of this form

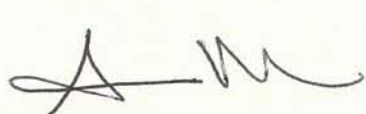
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Your name:	Mrs	Anna (first)	London (family)
Organisation:	Liverpool City Council		
Position:	Heritage Officer		
Daytime phone number:	02 9821 8856		
Fax number:	02 9821 9333		
Postal address:	Locked Bag 7064 (street address or postal box)		
	Liverpool BC (suburb or town)		
	NSW (state)	1871 (post code)	
Email address:	a.london@liverpool.nsw.gov.au		
Date form completed:	23 December 2013		

13. References used for completing this form

	Author	Title	Publisher	Repository / location	Year published
1.	Graham Brooks and Associates Pty Ltd	Heritage Assessment: Defence National Storage Distribution Centre (DNSDC), Moorebank Defence Site.	Prepared for the Department of Defence Sydney Property Disposals Unit, October 2002.		October 2002
2.	Artefact Heritage	Non-Indigenous Heritage: Sydney Intermodal Terminal Alliance (SIMTA) Transitional Part 3a Concept Plan Application	Prepared for the Sydney Intermodal Terminal Alliance		June 2013
3.					
4.					
5.					
6.					
7.					
8.					
9.					
10.					
11.					
12.					

14. Signed by author

 ANNA LONDON	Dated 23/12/13
Author (sign and print name)	

State Heritage Register Nomination Form

15. Signed by copyright holder(s) of image(s)

Image copyright holder (sign and print name)	Dated
Image copyright holder (sign and print name)	Dated
Image copyright holder (sign and print name)	Dated

Signing 14 and 15 confirms the author of this form and copyright holder(s) of images give permission for the Heritage Branch to use non-confidential information and images entered in this form for any purpose related to processing the nomination. Possible uses include (but are not limited to) publication on the Heritage Branch website, newsletter, reports or other publications. The author of the form, image copyright holder(s) and photographer(s) will be acknowledged when published.

I. Nominator

16. Nominator to be contacted by Heritage Branch

Name:	Mrs	Tanya (first)	O'Brien (family)
Organisation:	Liverpool City Council		
Position in organisation:	Manager Strategic Planning		
Daytime phone number:	02 9821 9341		
Fax number:	02 9821 9333		
Postal address:	Locked Bag 7064 (street address or postal box)		
	Liverpool BC (suburb or town)		
	NSW (state)		1871 (post code)
Email address:	t.obrien@liverpool.nsw.gov.au		

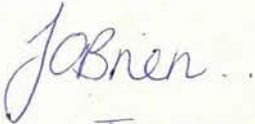
Alternate contact name:	Mrs	Anna (first)	London (family)
Position in organisation:	Heritage Officer		
Daytime phone number:	02 9821 8856		
Fax number:	02 9821 9333		
Postal address:	Locked Bag 7064, Liverpool BC NSW 1871		
Email address:	a.london@liverpool.nsw.gov.au		

State Heritage Register Nomination Form

17. Reasons for nomination

Background or reasons for nomination:	The DNSDC is a highly significant heritage place that is already recognised as being nationally significant by its listing on the CHL. Unfortunately as the site has been divested by the Department of Defence, the heritage protection for this place will be obsolete once the military use ceases. The reason for this nomination is to ensure the ongoing protection of this special heritage place through State Heritage Register listing.
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18. Form signed by nominator for submitting

 TANYA O'BRIEN	23-12-13
Nominator (sign and print name)	Dated

CHECKLIST



[Spell check your form - double click here \(or F7\)](#)

Before submitting this form, check that you have:

- ☒ completed all yellow sections, and other sections marked as essential.
- ☒ completed blue sections wherever possible.
- ☒ explained why the place or object is *state* significant in the statement of state significance.
- ☒ explained *how* one or more of the criterion has been fulfilled for listing.
- ☒ inserted photographs, maps and other illustrations as digital or scanned images.
- ☒ acknowledged all sources and references you used.
- ☐ signed and dated this form.
- ☒ obtained the nominator's and image copyright holders' signatures.
- ☐ if possible, attached a disk (or non-digital prints) for posting select image/s of high resolution, suitable for publication.

TO SUBMIT THIS NOMINATION:

Post all of the following: - a signed hard-copy of the form; - an electronic copy of the completed form saved to disk (unless you prefer to email it separately); and - other attachments, such as high-resolution copies of photographs suitable for publication.	Addressed to: The Director Heritage Branch NSW Department of Planning Locked Bag 5020 Parramatta NSW 2124 If an electronic copy of the form is not posted, email it instead to: nominations@planning.nsw.gov.au
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J. Additional photographs, maps or other images

Image 2

Photograph or
other image:



Subject of image:	Locations of former buildings (purple) in undeveloped areas of the DNSDC site (based on a 1966 plan of the site). Inset shows surviving slab of former store building.
Date of image:	2013
Photographer or author:	Artefact Heritage with Google Earth base map.
Copyright holder:	Artefact Heritage

Image 3

NSW GOVERNMENT
Department of Planning

Heritage Council
of New South Wales

Subject of image:	Plan of proposed layout of Moorebank Ordnance Depot.
Date of image:	25 April 1944
Photographer or author:	Artefact Heritage, 2013, p. 26, Figure 11. (National Archives of Australia: SP459/1, 420/7/1153)
Copyright holder:	

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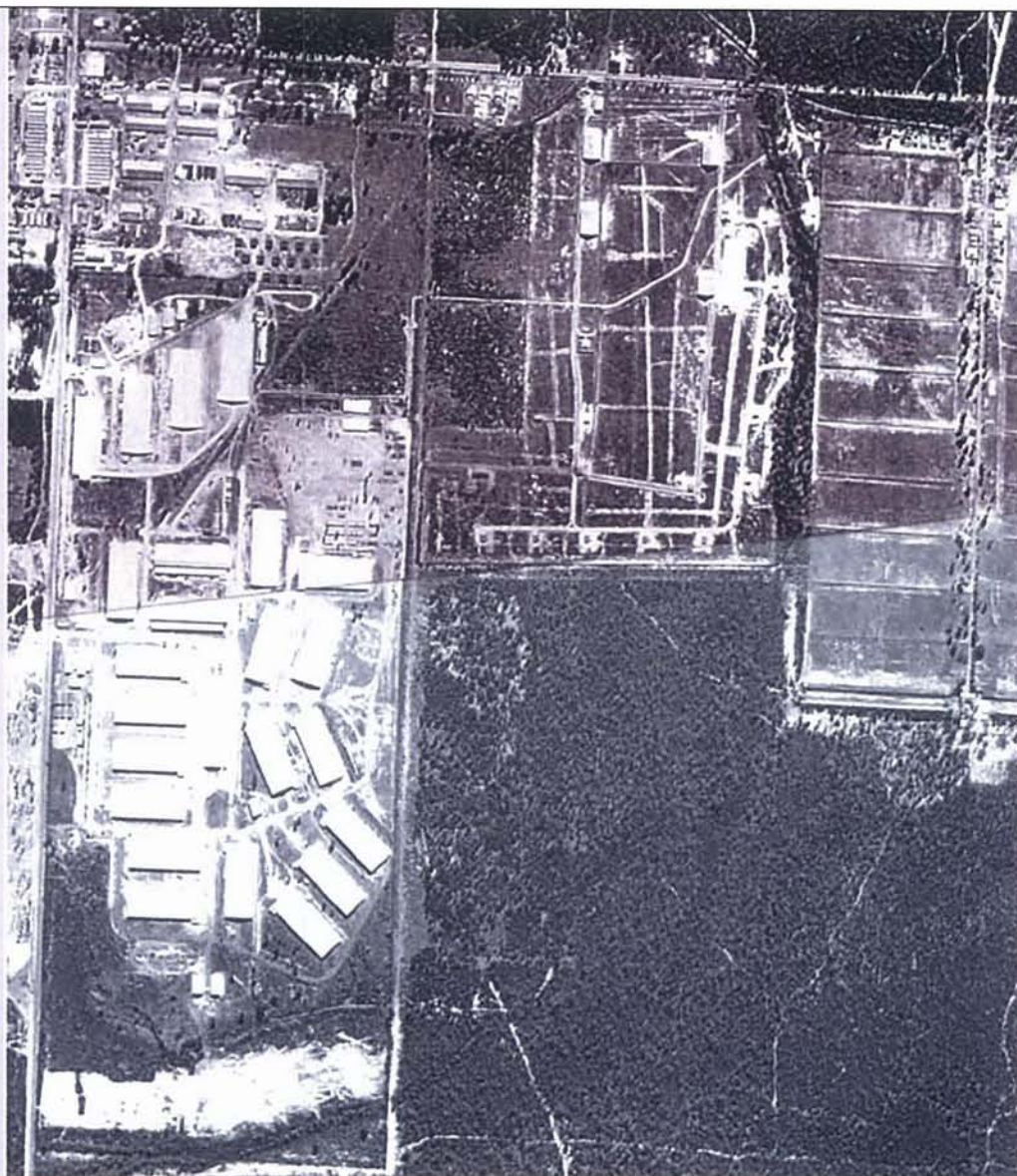
Image 4

Photograph or other image:	
Subject of image:	5th Australian Base Ordnance Depot. Exterior view of No. 9 Bulk (Crane Served) Technical Store Shed. This is one of two served by overhead electrical cranes. All heavy bulk equipment such as crated jeeps and ordnance parts such as artillery parts were handled from this shed.
Date of image:	23 January 1946
Photographer or author:	L. CPL E. MCQUILLAN
Copyright holder:	Australian War Memorial digital collection - ID No. 124623. Copyright expired.

Image 5

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Photograph
or other
image:



Subject of image:	Aerial photograph showing the Ordnance Depot/DNSDC site in 1951
Date of image:	1951
Photographer or author:	Graham Brooks & Associates, 2002, p.9, Figure 2.4.
Copyright holder:	Department of Lands NSW

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Image 6

Photograph or other image:		KEY Timber post and beam store buildings (9 bays wide) Composite timber and steel store buildings (crane served) Carpentry workshop QM (Quarter Master's) store Pre-1951 building, type unknown Modern buildings, c. 1990s Boundary of DNSDC site Boundary of SIMTA site
Subject of image:	Plan showing the current location of building types within the DSND C site.	
Date of image:	2013	
Photographer or author:	Artefact Heritage	
Copyright holder:	Artefact Heritage	

Image 7

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Photograph or other image:



Subject of image:	Building 9, at the centre of the DNSDC site - a typical WWII composite timber and steel warehouse building.
Date of image:	2002
Photographer or author:	Graham Brooks & Associates
Copyright holder:	Graham Brooks & Associates

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Image 8

Photograph or other image:	
Subject of image:	Interior of Building 79, showing original timber post and beam construction.
Date of image:	2013
Photographer or author:	Artefact Heritage
Copyright holder:	Artefact Heritage

Image 9

State Heritage Register Nomination Form

Photograph or other
image:



Subject of image: The visible railway siding to the south, opposite the current Buildings 17 & 18 (previously Buildings 14 & 15).

Date of image: 2013

Photographer or author: Artefact Heritage

Copyright holder: Artefact Heritage

State Heritage Register Nomination Form

Image 10

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

Image 11

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

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Image 12

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

Image 13

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

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Image 14

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

Image 15

Photograph or other image:	
Subject of image:	
Date of image:	
Photographer or author:	
Copyright holder:	

Planning Proposal

Liverpool Local Environmental Plan 2008

Draft Amendment Number 37

**Heritage Listing of the Defence National
Storage and Distribution Centre**

October 2013

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Background

This planning proposal has been drafted in accordance with Section 55 of the Environmental Planning and Assessment Act 1979 (the Act) and the Department of Planning and Infrastructure's guide to preparing planning controls.

The Defence National Storage and Distribution Centre (DNSDC) located on Moorebank Avenue, Moorebank was zoned industrial in 2004 to enable the development of the Moorebank Technology Park. This is however unlikely to occur as the site is proposed to be developed as an intermodal freight terminal.

The site is owned by SIMTA, a consortium comprising Aurizon Pty Ltd and Qube Pty Ltd and is currently leased to the Department of Defence.

Council has noted that the DNSDC was listed on the Commonwealth Heritage Register, as it is a "Commonwealth area".

However, once the Department of Defence lease ends, the site will no longer be a "Commonwealth area" and would need to be removed from the Commonwealth Heritage List in accordance with Section 341L *Environmental Protection and Biodiversity Conservation Act 1999*. From this point on the site would have no heritage listing and no heritage protection will be afforded to the site.

Both the Heritage Assessment prepared by Graham Brooks in 2001 and the Non Indigenous Heritage Impact Assessment prepared by artefact dated December 2012 recognise the high heritage significance of the DNSDC site, therefore Council is seeking to list the site in Schedule 5 of the Liverpool Local Environmental Plan 2008.

Delegation:

Council is seeking an Authorisation to make the plan.

Site identification

The planning proposal affects the Lot 1 DP1048263, Moorebank Avenue, Moorebank.

The site was part of the military land but was rezoned to become Moorebank Technology Park in 2004. However, the Department of Defence continue to lease the site.

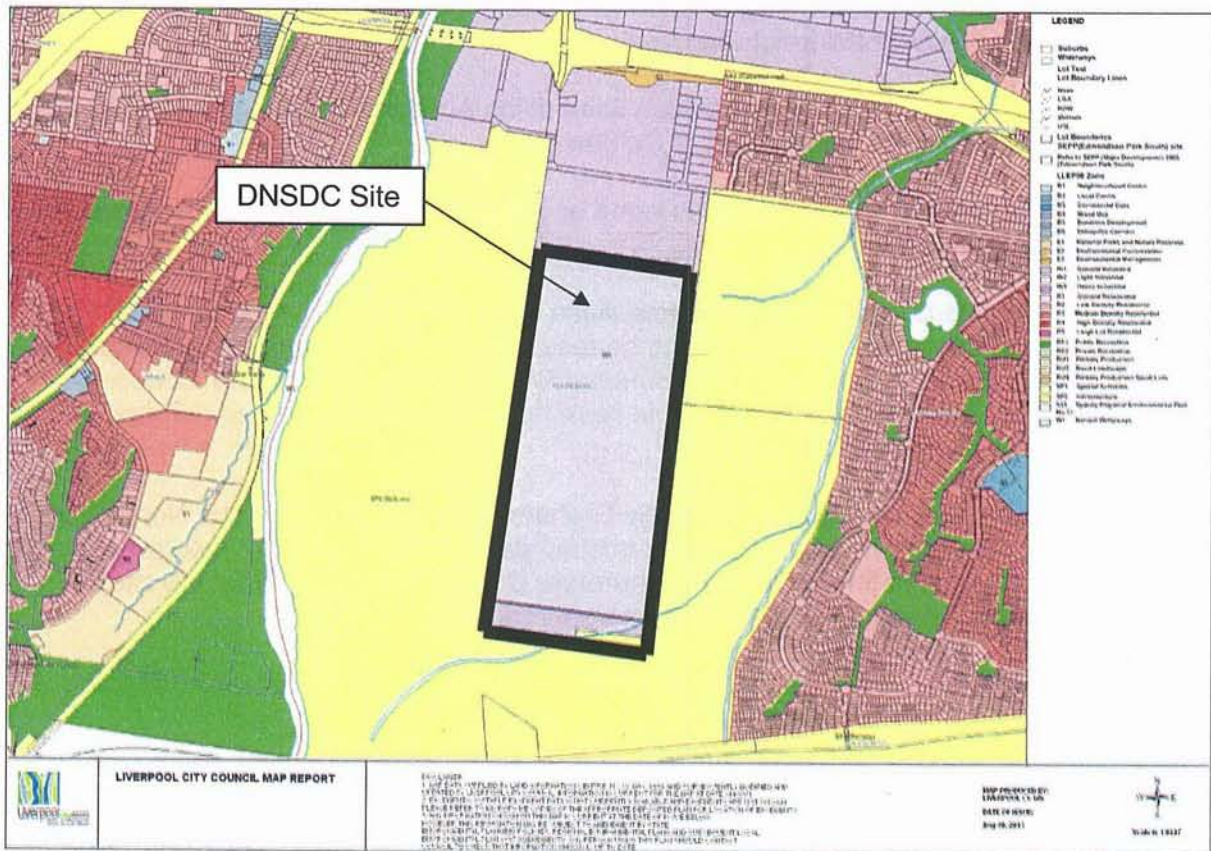


Figure 1 – Site Identification Map

Part 1 - Objectives

The objective of the planning proposal is to protect the heritage significance of the DNSDC site by listing the site on the local heritage register

Part 2 - Explanation of provisions

Amendment applies to:	Explanation of provision
Schedule 5 of the Liverpool LEP 2008.	Add a new item to the Schedule to list the DNSDC site on the local heritage register.
Heritage Map HER-013	Amend the map to identify the subject site as a heritage item.

Part 3 - Justification

A. Need for the planning proposal

1. Is the planning proposal a result of any strategic study or report?

The planning proposal is the result of a report. The Non Indigenous Heritage Impact Assessment prepared by artefact on behalf of SIMTA for the Part 3A application and EPBC Act approval recognised the heritage significance of the site and that there will be no heritage protection of the site once Commonwealth activities on the site cease.

The site contains twenty timber post and beam buildings dating to World War Two. Fifteen of these are of timber post and beam construction, with nine internal bays. Two other buildings are the smaller Quartermaster's Store, with five bays of timber post and beam construction, and the Carpentry Workshop, which are timber framed and three bays wide. Both the Quartermaster's Store and the Carpentry Workshop are constructed of Oregon, an American wood.

Other elements of significance include the alignments of rail sidings that once ran through the site which are still visible in the landscape through the location and orientation of some of the buildings and roads, while to the south a remaining siding is still clearly visible.

Finally, the section of the School of Military Engineering which is included in the subject site is disturbed and does not contain significant heritage items, the significance of the site as a whole should be considered. The School of Military Engineering is listed on the Liverpool LEP 2008 as the Australian Army Engineers Group (Item 57).

This listing notes that the site includes the Royal Australian Engineers (RAE) Memorial Chapel, RAE Monument, Major General Sir Clive Steele Memorial Gates, and The Cust Hut. According to the LEP Heritage map, Item 57 also encompasses most of the land surrounding the DNSDC site, between the East Hills railway line and Anzac Road, as well as a building on the north side of Anzac Road.

A *Heritage Assessment* of the site was prepared by Graham Brooks and Associates in 2001 on behalf of the Department of Defence. A *Non Indigenous Heritage Impact Assessment* was prepared by artefact dated December 2012 on behalf of SIMTA. Both these reports

recognise the high heritage significance of the DNSDC site and recommend preservation of the site.

There is substantial justification for the protection of the heritage significance of the site through the Liverpool Local Environmental Plan 2008.

In order to provide proper recognition of the built heritage items on site, it was considered by Council the best course of action to list the site on the local heritage register.

2. *Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?*

The intended outcome is to provide heritage protection to the DNSDC site. Council is encouraging the Office of Environment and Heritage to consider state listing, however Council believe the site has both state and local significance and should be listed on both registers.

The planning proposal is the best method of achieving the intended outcome, within Councils control.

3. *Will the net community benefit outweigh the cost of implementing and administering the planning proposal?*

The net community benefit will outweigh the cost of implementing and administering the plan. Given that the heritage studies have already been prepared by the private sector there is little cost associated with the implementation and administration of the planning proposal.

There will be a net community benefit in the recognition of the heritage significance of the DNSDC site at a local level.

B. Relationship to strategic planning framework

4. *Is the planning proposal consistent with the objectives and actions contained within the applicable regional or sub-regional strategy (including the Sydney Metropolitan Strategy and exhibited draft strategies)?*

Sydney Metropolitan Strategy

The planning proposal is generally consistent with the Sydney Metropolitan Strategy. The strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

Draft Metropolitan Strategy for Sydney to 2031

The planning proposal is generally consistent with the Draft Metropolitan Strategy for Sydney to 2031. The strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

South West Draft Subregional Strategy

The draft South West Subregional strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

5. Is the planning proposal consistent with the local council's Community Strategic Plan, or other local strategic plan?

The Planning Proposal is consistent with the aims and objectives of Growing Liverpool 2023.

6. Is the planning proposal consistent with the applicable state environmental planning policies?

The planning proposal is consistent with State Environmental Planning Policies.

7. Is the planning proposal consistent with applicable Ministerial Directions (s.117 directions)?

Various Section 117 Ministerial Directions apply to this planning proposal. These are discussed below:

S.117 Direction No. and Title	Contents of S.117 Direction	Planning Proposal	Comply
1.1 Business and Industrial Zones	The objective of this direction is to encourage employment growth in suitable locations, protect employment land in business and industrial zones and support the viability of identified strategic centres.	The planning proposal affects land zoned industrial but does not prohibit industrial development on the site.	Yes
2.3 Heritage Conservation	The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.	The planning proposal is consistent with this direction as it directly seeks to conserve a place of environmental heritage.	Yes
5.1 Implementation of Regional Strategies	The objective of this direction is to give legal effect to the vision, land use strategy, policies, outcomes and actions contained in regional strategies. This direction requires that the rezoning proposal be consistent with the South West Subregional Strategy.	The Planning Proposal is consistent with the relevant Regional Strategies.	Yes
7.0 Metropolitan Planning	Planning proposals shall be consistent with the NSW Government's Metropolitan Plan for Sydney 2036 published in December 2010.	The planning proposal achieves the overall intent of the Plan and does not undermine the achievement of its vision, land use strategy, policies, outcomes or actions.	Yes

C. Environmental, social and economic impact

8. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

No critical habitat or threatened species, populations or ecological communities or their habitats will be adversely affected as a result of the proposal.

9. *Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?*

The planning proposal will not result in any negative environmental effects.

10. *How has the planning proposal adequately addressed any social and economic effects?*

The planning proposal has no negative social or economic effects. The planning proposal recognises the cultural and heritage significance of the site. No negative impacts are envisaged.

D. State and Commonwealth interests

11. *Is there adequate public infrastructure for the planning proposal?*

The planning proposal does not result in any additional development. As such the planning proposal will not place additional demands on public infrastructure.

12. *What are the views of State and Commonwealth public authorities consulted in accordance with the Gateway determination?*

State and Commonwealth agency consultation will occur following the issue of a Gateway determination.

Part 4 – Maps

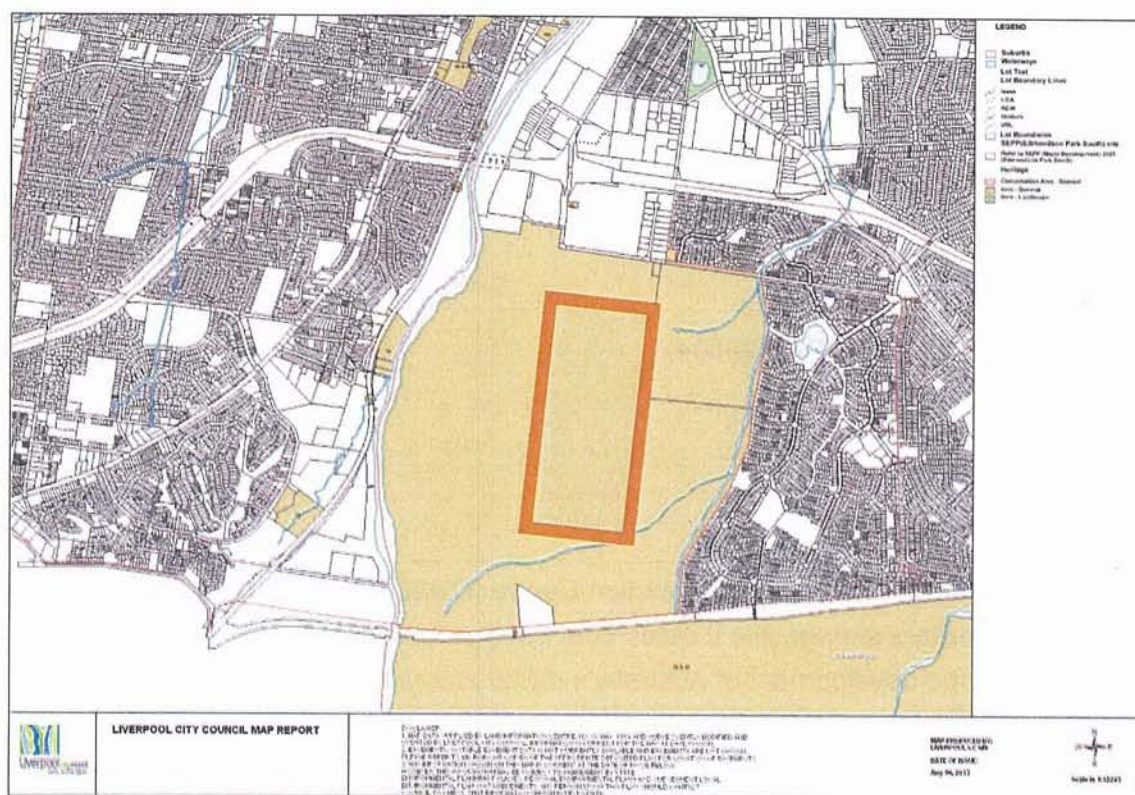


Figure 2 – Draft Heritage Map

Part 5 - Community Consultation

Community consultation will occur following the issue of a Gateway determination.

Council does not seek approval to issue a summary of the Planning Proposal for the purposes of community consultation.

Part 6 - Timeline

Council estimates that a timeframe of 6 months, from the issue of a Gateway determination, is required to complete the process.

Timeframe	Action
15 November 2013	Receive Gateway Determination
20 November – 11 December 2013	Public authority consultation
21 December - 21 February 2013	Public exhibition
22 February 2013 – 22 March 2014	Review of submissions
March 2014	Report to Council and finalisation of LEP

Appendix 1 Commonwealth Heritage Register Listing

Defence National Storage and Distribution Centre, Moorebank Av, Moorebank, NSW, Australia

Photographs	None
List	Commonwealth Heritage List
Class	Historic
Legal Status	Listed place (22/06/2004)
Place ID	105641
Place File No	1/15/023/0020

Summary Statement of Significance

The Defence National Storage and Distribution Centre (DNSDC) is historically highly significant. As a military storage site it dates from 1915, and the Centre is important for its associations with the development of Australia's military forces prior to and during the First World War and particularly for its direct association with the military build up in the early years of the Second World War. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The place also has an association with early nineteenth century settlement in the Liverpool area. (Criterion A.4) (Themes: 7.7 Defending Australia, 7.7.1 Providing for the common defence, 7.7.3 Going to war)

The DNSDC contains twenty Second World War post and beam warehouses, many of which, despite being re-clad, are good examples of their type. Particularly important are the fifteen timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Also important are the three composite steel and timber type warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s. Further, the alignment of part of the former military railway system is evidenced by the alignment and siting of some of the buildings and roads at the site. (Criteria D.2 and B.2)

The Centre is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site. (Criterion G.1)

Official Values

Criterion A Processes

The Defence National Storage and Distribution Centre (DNSDC) is historically highly significant. As a military storage site it dates from 1915, and the Centre is important for its associations with the development of Australia's military forces prior to and during the First World War and particularly for its direct association with the military build up in the early years of the Second World War. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The place also has an association with early nineteenth century settlement in the Liverpool area.

Attributes

The fabric, form and set out of all of the buildings, including their alignments and orientations to former roads and railways.

Criterion B Rarity

Particularly important are the fifteen timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value.

Attributes

The post and beam warehouses.

Criterion D Characteristic values

The DNSDC contains twenty Second World War post and beam warehouses, many of which, despite being re-clad, are good examples of their type. In addition to the fifteen timber post and beam military warehouses of the nine-bay type are the three composite steel and timber type warehouses, which are also important. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s. Further, the alignment of part of the former military railway system is evidenced by the alignment and siting of some of the buildings and roads at the site.

Attributes

The fabric, form and set out of all of the buildings, including their alignments and orientations to former roads and railways.

Criterion G Social value

The Centre is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site.

Attributes
Not clarified.

Description

HISTORY

The Defence National Storage and Distribution Centre (DNSDC) at Moorebank, which is part of the larger Moorebank Defence site, stands within part of the original 6000 acre property granted to Thomas Moore in 1805-21. The eastern boundary of the DNSDC site follows the alignment of part of Moore's property. Moore was the first magistrate at Liverpool and was an influential figure in the early years of the area. The northern and western boundaries of the DNSDC follow boundaries of the later 1888 Moorebank Farms subdivision.

Meanwhile, military activities began in the Liverpool area early in the 1800s when troops guarded convicts and a military barracks was constructed in the town. Military use of the area increased early in the twentieth century, and training camps were run. Military manoeuvres were held for the inspection of Lord Kitchener in 1910, and a rifle range was developed. The military camp was located partly on the present DNSDC site.

One result of Kitchener's visit was the acquisition by the Commonwealth of large areas of land for military purposes, and in 1913 an area exceeding 16,000 acres and including the DNSDC site was acquired by the federal government for defence purposes. This was the era of compulsory training and permanent military training areas, hence the Commonwealth's need to acquire large areas. Military storage at the DNSDC appears to have begun in 1915.

The Liverpool camp was the main military training centre in NSW during the First World War. All new recruits came here, including Light Horse units. In 1919 two railway sidings were added in order to service the depot, and were part of the larger railway system which linked the various military establishments in the area. One of the sidings was extended during the Second World War.

As mentioned above, military storage began at what is now called the DNSDC in 1915, during the First World War. Called initially the Mobilisation Stores, the place accommodated the 2nd Base Ordnance Depot and 2nd Base Workshops during the Second World War. In 1972 these units became the 21st Supply Battalion and the 2nd Base Workshop Battalion. In 1990 these two joined to become the Moorebank Logistics Group.

During the Second World War there were perhaps 20 timber post and beam buildings erected, as well as other buildings. All these structures reflected the widespread expansion of support facilities during the early part of the war.

From the 1940s to the early 1990s there was little development at the site. Then in the early 1990s the place was upgraded to become the DNSDC. Most of the Second World War buildings were reclad and the original asbestos cement sheeting was removed. A small number of buildings was demolished and replaced with more modern designs. The creation of the DNSDC was an element of the Defence Logistics Redevelopment Project which aimed at rationalising Defence warehousing. DNSDC is the main warehouse for all three of the Australian armed services, and there are maintenance sections as well.

DESCRIPTION

The place is mainly made up of a number of large storage sheds, many dating from the early 1940s, which have been variously extended and refurbished as part of the early 1990s work. There is also a number of smaller ancillary, administration and workshop buildings. Near the main entrance stands the main headquarters and administration building for the DNSDC.

Twenty Second World War timber post and beam buildings of various configurations appear to remain, all having been reclad. Of these, 15 (building numbers 6, 10-11, 33-35, 39-40, 44-46, 48, 72-73 and 75) are of timber post and beam construction, with 9 internal bays (the widest type built). With their original timber structure intact, they have been reclad with modern profiled steel sheeting, have concrete floors, and sprinklers and smoke management screens. Three of the 20 buildings (7, 9, 80) are composite timber and steel warehouse structures, with three bays of timber post and beam construction on either side of a central raised bay, steel-framed to support a gantry crane. The other two buildings are the QM Store (13) five bays wide, and the Carpentry Workshop (37) three bays wide and extended.

The timber used in the 20 post and beam buildings is Oregon. It is understood that the buildings were prefabricated in the United States and shipped to Australia on US Liberty Ships.

Post and beam stores buildings were built during the Second World War in numbers of military establishments in eastern Australia, and particularly in south-eastern Australia. They played a major military warehousing role at the time. In 1995 a number remained in use, including at Bogan Gate, Regents Park and Wallangarra in NSW and at Broadmeadows and Bandiana Victoria. However, these types of buildings are now fairly rare, for by 2002 the only other known site with similar Second World War timber store buildings still in Defence ownership, was at Bandiana. Hence the examples at DNSDC have rarity value as well as being good examples of the type (particularly in the case of the nine-bay types).

The alignment of part of the former Liverpool - Anzac Rifle Range - Holsworthy military railway line and sidings can still be seen via the alignment and siting of some of the buildings and roads at the site. The Ordnance Depot railway siding constructed in 1919 and extended

during the Second World War has been mainly demolished.

History Not Available

Condition and Integrity

The timber post and beam buildings at DNSDC were reclad in the early 1990s. The DNSDC is in the course of being disposed of by Defence. (October 2002)

Location

About 83ha, at Moorebank, being an area bounded by a line commencing at the intersection of Moorebank Avenue with an unnamed road at approximate AMG point 307820mE 6240100mN, then easterly and northerly via the unnamed road to its intersection with Anzac Road and Greenhills Avenue, then westerly via Anzac Road to its intersection with Moorebank Avenue, then southerly via Moorebank Avenue to the point of commencement.

Bibliography

Graham Brooks and Associates Pty Ltd, Heritage Assessment: Defence National Storage Distribution Centre (DNSDC), Moorebank Defence Site. Prepared for the Department of Defence Sydney Property Disposals Unit, October 2002.

Nolan, Gregory, Australian Timber Buildings of the Second World War.
www.oak.arch.utas.edu.au/projects/retro/war/rwar.html 2002.

Evaluation Criteria for the Delegation of Plan Making Functions

Local Government Area:

Liverpool City Council

Name of the draft LEP:

Liverpool LEP 2008 (draft Amendment No. 37)

Address of Land:

Lot 1 DP1048263, Moorebank Avenue, Moorebank

Intent of draft LEP:

To include the subject site in Schedule 5 of the Liverpool LEP 2008 as a local heritage item.

Additional Supporting Points/Information:

It is noted that the site is likely to no longer have any heritage listing afforded to it once the Department of Defence lease ends. Council is seeking to make planning instruments and heritage listing consistent.

Evaluation Criteria for the Issuing of an Authorisation

(NOTE – where the matter is identified as relevant and the requirement has not been met, council is attach information to explain why the matter has not been addressed)	Council Response		Department Assessment	
	Y/N	Not Relevant	Agree	Not Agree
Is the planning proposal consistent with the Standard Instrument Order, 2006?	Y			
Does the planning proposal contain an adequate explanation of the intent, objectives, and intended outcome of the proposed amendment?	Y			
Are appropriate maps included to identify the location of the site and the intent of the amendment?	Y			
Does the planning proposal contain details related to proposed consultation?	Y			
Is the planning proposal compatible with an endorsed regional or sub-regional planning strategy or a local strategy endorsed by the Director-General?	Y			
Does the planning proposal adequately address any consistency with all relevant S117 Planning Directions?	Y			
Is the planning proposal consistent with all relevant State Environmental Planning Policies (SEPPs)?	Y			

Minor Mapping Error Amendments	Y/N			
Does the planning proposal seek to address a minor mapping error and contain all appropriate maps that clearly identify the error and the manner in which the error will be addressed?		N/A		
Heritage LEPs	Y/N			
Does the planning proposal seek to add or remove a local heritage item and is it supported by a strategy/study endorsed by the Heritage Office?	See attachment			
Does the planning proposal include another form of endorsement or support from the Heritage Office if there is no supporting strategy/study?	See attachment			
Does the planning proposal potentially impact on an item of State Heritage Significance and if so, have the views of the Heritage Office been obtained?	See attachment			
Reclassifications	Y/N			
Is there an associated spot rezoning with the reclassification?		N/A		
If yes to the above, is the rezoning consistent with an endorsed Plan of Management (POM) or strategy?		N/A		
Is the planning proposal proposed to rectify an anomaly in a classification?		N/A		
Will the planning proposal be consistent with an adopted POM or other strategy related to the site?		N/A		
Will the draft LEP discharge any interests in public land under section 30 of the <i>Local Government Act, 1993</i> ?		N/A		
If so, has council identified all interests; whether any rights or interests will be extinguished; any trusts and covenants relevant to the site; and, included a copy of the title with the planning proposal?		N/A		
Has the council identified that it will exhibit the planning proposal in accordance with the department's Practice Note (PN 09-003) <i>Classification and reclassification of public land through a local environmental plan and Best Practice Guideline for LEPs and Council Land</i> ?		N/A		
Has council acknowledged in its planning proposal that a Public Hearing will be required and agreed to hold one as part of its documentation?		N/A		
Spot Rezonings	Y/N			

Will the proposal result in a loss of development potential for the site (ie reduced FSR or building height) that is not supported by an endorsed strategy?		N/A		
Is the rezoning intended to address an anomaly that has been identified following the conversion of a principal LEP into a Standard Instrument LEP format?		N/A		
matter in an existing LEP and if so, does it provide enough information to explain how the issue that lead to the deferral has been addressed?		N/A		
If yes, does the planning proposal contain sufficient documented justification to enable the matter to proceed?		N/A		
Does the planning proposal create an exception to a mapped development standard?		N/A		
Section 73A matters				
Does the proposed instrument a. correct an obvious error in the principal instrument consisting of a misdescription, the inconsistent numbering of provisions, a wrong cross-reference, a spelling error, a grammatical mistake, the insertion of obviously missing words, the removal of obviously unnecessary words or a formatting error?; b. address matters in the principal instrument that are of a consequential, transitional, machinery or other minor nature?; or c. deal with matters that do not warrant compliance with the conditions precedent for the making of the instrument because they will not have any significant adverse impact on the environment or adjoining land? (NOTE – the Minister (or Delegate) will need to form an Opinion under section 73(A)(1)(c) of the Act in order for a matter in this category to proceed).	N			

Attachment:

The Planning Proposal includes the heritage listing of the subject site. Council is concurrently nominating the site for State Heritage Listing. It is noted that the Draft Statement of Commitments in the Environmental Assessment currently on exhibition for the SIMTA proposal, the proponent is committed to commence discussions with the appropriate heritage bodies regarding potential listing of the DNSDC site on the National Heritage List or the State Heritage Register.

Does the planning proposal seek to add or remove a local heritage item and is it supported by a strategy/study endorsed by the Heritage Office?	The planning proposal seeks to add a local heritage item. The proposal is not formally endorsed by the Heritage Office, however Council is nominating the site for the State Heritage List, to align the local and state lists with the Commonwealth Heritage List.
Does the planning proposal include another form of endorsement or support from the Heritage Office if there is no supporting strategy/study?	As stated above, Council is nominating the site for the State Heritage List, as well as proposing to list the site on the local heritage register. This will create consistency with the Commonwealth Heritage List and promote local heritage.
Does the planning proposal potentially impact on an item of State Heritage Significance and if so, have the views of the Heritage Office been obtained?	The planning proposal will have a positive impact on the heritage items in the vicinity. By including additional heritage places on the local heritage register, the heritage significance of the precinct will be enhanced and protected into the future.

GMPG 04

ITEM NO: GMPG 04
FILE NO: 2013/2622
SUBJECT: HERITAGE LISTING OF THE DEFENCE NATIONAL STORAGE AND
DISTRIBUTION CENTRE SITE

RECOMMENDATION

That Council:

1. Forwards the Planning Proposal to add the Defence National Storage and Distribution Centre to the Liverpool Local Environmental Plan 2008 as a heritage item to the Department of Planning and Infrastructure for a Gateway determination.
2. Writes to the Office of Environment and Heritage and encourage them to list the site on the State Heritage Register.

COUNCIL DECISION

Motion: **Moved: Cllr Hadchiti** **Seconded: Cllr Mamone**

That the recommendation be adopted.

On being put to the meeting the motion was declared CARRIED.

Councillors voted unanimously for this motion.

LIVERPOOL CITY COUNCIL

PLANNING AND GROWTH

ORDINARY MEETING

25/09/2013

ITEM NO:	GMPG 04	FILE NO:	2013/2622
SUBJECT:	HERITAGE LISTING OF THE DEFENCE NATIONAL STORAGE AND DISTRIBUTION CENTRE SITE		
COMMUNITY STRATEGIC PLAN REFERENCE:	VIBRANT PROSPEROUS CITY		

EXECUTIVE SUMMARY:

The Defence National Storage and Distribution Centre (DNSDC) is located on Moorebank Avenue, Moorebank and is proposed to be developed as an intermodal freight terminal by SIMTA. The site is currently leased to the Department of Defence and due to this lease arrangement the site is therefore known as a "Commonwealth area". The site includes a number of heritage elements which have been acknowledged by heritage reporting. The site is currently listed on the Commonwealth Heritage Register.

However, once the Department of Defence lease ends, the site will no longer be a "Commonwealth area" and would need to be removed from the Commonwealth Heritage List. From that point on the site would have no heritage listing and protection of the heritage values on the site.

In response Council has prepared a Planning Proposal to list the DNSDC site on the Local Heritage Register in the Liverpool Local Environmental Plan 2008 to ensure ongoing protection of the heritage of the site is provided.

It is recommended that Council forward the Planning Proposal to add the DNSDC to the Liverpool Local Environmental Plan 2008 as a heritage item to the Department of Planning and Infrastructure for a Gateway determination.

Further, it is recommended that Council write to the Office of Environment and Heritage and encourage them to list the site on the State Heritage Register.

DETAILED REPORT:

Site Identification

The planning proposal affects the Lot 1 DP1048263, the DNSDC site on Moorebank Avenue, Moorebank, otherwise known as the SIMTA intermodal freight terminal site.

The site was previously part of the military land but was rezoned to become Moorebank Technology Park in 2004. However the Department of Defence continue to lease the site. The site is proposed in the future to be developed as an intermodal freight terminal.



Figure 1 - Site identification map

Planning Proposal

Council has prepared a Planning Proposal which seeks to list the DNSDC site on the Local Heritage Register in the Liverpool Local Environmental Plan 2008. The Planning Proposal outlines the technical planning justification for the listing.

The DNSDC is listed on the Commonwealth Heritage Register, as it is a "Commonwealth area" because the Department of Defence still leases the site. However, once the Department of Defence lease ends, the site will no longer be a "Commonwealth area" and would need to be removed from the Commonwealth Heritage List in accordance with Section 341L *Environmental Protection and Biodiversity Conservation Act 1999*. From that point on the site would have no heritage listing and no heritage protection will be afforded to the site.

The ongoing heritage protection measures that will exist once Defence vacates the DNSDC site will entirely depend on any contractual obligations between SIMTA and the Department of Defence that operate at that point in time (however given that the

Department of Defence is giving up the site, these may not be stringent in relation to heritage).

Heritage Significance of the DNSDC site

The site contains a variety of heritage buildings and a long heritage association. In particular the site has twenty timber post and beam buildings dating to World War Two. Fifteen of these are of timber post and beam construction, with nine internal bays. Two other buildings are the smaller Quartermaster's Store, with five bays of timber post and beam construction, and the Carpentry Workshop, which are timber framed and three bays wide. Both the Quartermaster's Store and the Carpentry Workshop are constructed of Oregon, an American wood.

Other elements of significance include the alignments of rail sidings that once ran through the site which are still visible in the landscape through the location and orientation of some of the buildings and roads, while to the south a remaining siding is still clearly visible.

The section of the School of Military Engineering which is included in the subject site is disturbed and does not contain significant heritage items, however the significance of the site as a whole should be considered. The School of Military Engineering is listed on the Liverpool LEP 2008 as the Australian Army Engineers Group (Item 57).

This listing notes that the site includes the Royal Australian Engineers (RAE) Memorial Chapel, RAE Monument, Major General Sir Clive Steele Memorial Gates, and The Cust Hut. According to the LEP Heritage map, Item 57 also encompasses most of the land surrounding the DNSDC site, between the East Hills railway line and Anzac Road, as well as a building on the north side of Anzac Road. As such the proposed listing will complement this existing listing and provide a more comprehensive heritage consideration to this area.

A *Heritage Assessment* of the site was prepared by Graham Brooks and Associates in 2001 on behalf of the Department of Defence. A *Non Indigenous Heritage Impact Assessment* was prepared by Artefact dated December 2012 on behalf of SIMTA. Both these reports recognise the high heritage significance of the DNSDC site and recommend preservation of the site.

There is substantial justification for the protection of the heritage significance of the site through the Liverpool Local Environmental Plan 2008.

Office of Environment and Heritage

The NSW Office of Environment and Heritage administers the State Heritage Register. Given that the DNSDC site was considered significant enough to be listed on the Commonwealth Heritage Register, it seems appropriate that the site be listed on the State Heritage Register as well as listed locally.

It is recommended that Council write to the Office of Environment and Heritage and encourage them to list the site on the State Heritage Register in conjunction with local listing.

FINANCIAL IMPLICATIONS:

The reports required to justify the heritage listing are existing and publically available. The recommended actions will require some staff time to prepare and liaise with relevant state agencies, which can be managed through existing budget allocations.

The planning proposal provides public benefit through provision of better protection to significant heritage items. There are no substantial financial implications arising from this report.

RECOMMENDATION:

That Council:

1. Forwards the Planning Proposal to add the Defence National Storage and Distribution Centre to the Liverpool Local Environmental Plan 2008 as a heritage item to the Department of Planning and Infrastructure for a Gateway determination.
2. Writes to the Office of Environment and Heritage and encourage them to list the site on the State Heritage Register.

SIGNED BY:

Toni Averay
Group Manager
Planning and Growth

Attachment: Planning Proposal



Planning Proposal

Liverpool Local Environmental Plan 2008

**Heritage Listing of the Defence National
Storage and Distribution Centre**

August 2013

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Background

This planning proposal has been drafted in accordance with Section 55 of the Environmental Planning and Assessment Act 1979 (the Act) and the Department of Planning and Infrastructure's guide to preparing planning controls.

The Defence National Storage and Distribution Centre (DNSDC) located on Moorebank Avenue, Moorebank was zoned industrial in 2004 to enable the development of the Moorebank Technology Park. This is however unlikely to occur as the site is proposed to be developed as an intermodal freight terminal.

The site is owned by SIMTA, a consortium comprising Aurizon Pty Ltd and Qube Pty Ltd and is currently leased to the Department of Defence.

Council has noted that the DNSDC was listed on the Commonwealth Heritage Register, as it is a "Commonwealth area".

However, once the Department of Defence lease ends, the site will no longer be a "Commonwealth area" and would need to be removed from the Commonwealth Heritage List in accordance with Section 341L *Environmental Protection and Biodiversity Conservation Act 1999*. From this point on the site would have no heritage listing and no heritage protection will be afforded to the site.

Both the Heritage Assessment prepared by Graham Brooks in 2001 and the Non Indigenous Heritage Impact Assessment prepared by artefact dated December 2012 recognise the high heritage significance of the DNSDC site, therefore Council is seeking to list the site in Schedule 5 of the Liverpool Local Environmental Plan 2008.

Delegation:

Council is seeking an Authorisation to make the plan.

Site Identification

The planning proposal affects the Lot 1 DP1048263, Moorebank Avenue, Moorebank.

The site was part of the military land but was rezoned to become Moorebank Technology Park in 2004. However, the Department of Defence continue to lease the site.



Figure 1 – Site Identification Map

Part 1 - Objectives

The objective of the planning proposal is to protect the heritage significance of the DNSDC site by listing the site on the local heritage register.

Part 2 - Explanation of provisions

Amendment applies to:	Explanation of provision
Schedule 5 of the Liverpool LEP 2008.	Add a new item to the Schedule to list the DNSDC site on the local heritage register.
Heritage Map HER-013	Amend the map to identify the subject site as a heritage item.

Part 3 - Justification

A. Need for the planning proposal

1. Is the planning proposal a result of any strategic study or report?

The planning proposal is the result of a report. The Non Indigenous Heritage Impact Assessment prepared by artefact on behalf of SIMTA for the Part 3A application and EPBC Act approval recognised the heritage significance of the site and that there will be no heritage protection of the site once Commonwealth activities on the site cease.

The site contains twenty timber post and beam buildings dating to World War Two. Fifteen of these are of timber post and beam construction, with nine internal bays. Two other buildings are the smaller Quartermaster's Store, with five bays of timber post and beam construction, and the Carpentry Workshop, which are timber framed and three bays wide. Both the Quartermaster's Store and the Carpentry Workshop are constructed of Oregon, an American wood.

Other elements of significance include the alignments of rail sidings that once ran through the site which are still visible in the landscape through the location and orientation of some of the buildings and roads, while to the south a remaining siding is still clearly visible.

Finally, the section of the School of Military Engineering which is included in the subject site is disturbed and does not contain significant heritage items, the significance of the site as a whole should be considered. The School of Military Engineering is listed on the Liverpool LEP 2008 as the Australian Army Engineers Group (Item 57).

This listing notes that the site includes the Royal Australian Engineers (RAE) Memorial Chapel, RAE Monument, Major General Sir Clive Steele Memorial Gates, and The Cust Hut. According to the LEP Heritage map, Item 57 also encompasses most of the land surrounding the DNSDC site, between the East Hills railway line and Anzac Road, as well as a building on the north side of Anzac Road.

A *Heritage Assessment* of the site was prepared by Graham Brooks and Associates in 2001 on behalf of the Department of Defence. A *Non Indigenous Heritage Impact Assessment* was prepared by artefact dated December 2012 on behalf of SIMTA. Both these reports

recognise the high heritage significance of the DNSDC site and recommend preservation of the site.

There is substantial justification for the protection of the heritage significance of the site through the Liverpool Local Environmental Plan 2008.

In order to provide proper recognition of the built heritage items on site, it was considered by Council the best course of action to list the site on the local heritage register.

2. *Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?*

The intended outcome is to provide heritage protection to the DNSDC site. Council is encouraging the Office of Environment and Heritage to consider state listing, however Council believe the site has both state and local significance and should be listed on both registers.

The planning proposal is the best method of achieving the intended outcome, within Councils control.

3. *Will the net community benefit outweigh the cost of implementing and administering the planning proposal?*

The net community benefit will outweigh the cost of implementing and administering the plan. Given that the heritage studies have already been prepared by the private sector there is little cost associated with the implementation and administration of the planning proposal.

There will be a net community benefit in the recognition of the heritage significance of the DNSDC site at a local level.

B. Relationship to strategic planning framework

4. *Is the planning proposal consistent with the objectives and actions contained within the applicable regional or sub-regional strategy (including the Sydney Metropolitan Strategy and exhibited draft strategies)?*

Sydney Metropolitan Strategy

The planning proposal is generally consistent with the Sydney Metropolitan Strategy. The strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

Draft Metropolitan Strategy for Sydney to 2031

The planning proposal is generally consistent with the Draft Metropolitan Strategy for Sydney to 2031. The strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

South West Draft Subregional Strategy

The draft South West Subregional strategy recognises the importance of heritage. This planning proposal seeks to create a new heritage listing.

5. Is the planning proposal consistent with the local council's Community Strategic Plan, or other local strategic plan?

The Planning Proposal is consistent with the aims and objectives of Growing Liverpool 2023.

6. Is the planning proposal consistent with the applicable state environmental planning policies?

The planning proposal is consistent with State Environmental Planning Policies.

7. Is the planning proposal consistent with applicable Ministerial Directions (s.117 directions)?

Various Section 117 Ministerial Directions apply to this planning proposal. These are discussed below:

S.117 Direction No. and Title	Contents of S.117 Direction	Planning Proposal	Comply
1.1 Business and Industrial Zones	The objective of this direction is to encourage employment growth in suitable locations, protect employment land in business and industrial zones and support the viability of identified strategic centres.	The planning proposal affects land zoned industrial but does not prohibit industrial development on the site.	Yes
2.3 Heritage Conservation	The objective of this direction is to conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.	The planning proposal is consistent with this direction as it directly seeks to conserve a place of environmental heritage.	Yes
5.1 Implementation of Regional Strategies	The objective of this direction is to give legal effect to the vision, land use strategy, policies, outcomes and actions contained in regional strategies. This direction requires that the rezoning proposal be consistent with the South West Subregional Strategy.	The Planning Proposal is consistent with the relevant Regional Strategies.	Yes
7.0 Metropolitan Planning	Planning proposals shall be consistent with the NSW Government's Metropolitan Plan for Sydney 2036 published in December 2010.	The planning proposal achieves the overall intent of the Plan and does not undermine the achievement of its vision, land use strategy, policies, outcomes or actions.	Yes

C. Environmental, social and economic impact

8. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

No critical habitat or threatened species, populations or ecological communities or their habitats will be adversely affected as a result of the proposal.

9. Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?

The planning proposal will not result in any negative environmental effects.

10. How has the planning proposal adequately addressed any social and economic effects?

The planning proposal has no negative social or economic effects. The planning proposal recognises the cultural and heritage significance of the site. No negative impacts are envisaged.

D. State and Commonwealth interests

11. Is there adequate public infrastructure for the planning proposal?

The planning proposal does not result in any additional development. As such the planning proposal will not place additional demands on public infrastructure.

12. What are the views of State and Commonwealth public authorities consulted in accordance with the Gateway determination?

State and Commonwealth agency consultation will occur following the issue of a Gateway determination.

Part 4 – Maps



Figure 2 – Draft Heritage Map

Part 5 - Community Consultation

Community consultation will occur following the issue of a Gateway determination.

Council does not seek approval to issue a summary of the Planning Proposal for the purposes of community consultation.

Part 6 - Timeline

Council estimates that a timeframe of 7 months, from the issue of a Gateway determination, is required to complete the process.

Timeframe	Action
15 October 2013	Receive Gateway Determination
20 October – 11 November 2013	Public authority consultation
21 November – 21 December 2013	Public exhibition
22 December 2013 – 22 January 2014	Review of submissions
February – March 2014	Report to Council and finalisation of LEP

Appendix 1 Commonwealth Heritage Register Listing

Defence National Storage and Distribution Centre, Moorebank Av. Moorebank, NSW, Australia

Photographs	None
List	Commonwealth Heritage List
Class	Historic
Legal Status	Listed place (22/06/2004)
Place ID	105641
Place File No	1/15/023/0020

Summary Statement of Significance

The Defence National Storage and Distribution Centre (DNSDC) is historically highly significant. As a military storage site it dates from 1915, and the Centre is important for its associations with the development of Australia's military forces prior to and during the First World War and particularly for its direct association with the military build up in the early years of the Second World War. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The place also has an association with early nineteenth century settlement in the Liverpool area. (Criterion A.4) (Themes: 7.7 Defending Australia, 7.7.1 Providing for the common defence, 7.7.3 Going to war)

The DNSDC contains twenty Second World War post and beam warehouses, many of which, despite being re-clad, are good examples of their type. Particularly important are the fifteen timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Also important are the three composite steel and timber type warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s. Further, the alignment of part of the former military railway system is evidenced by the alignment and siting of some of the buildings and roads at the site. (Criteria D.2 and B.2)

The Centre is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site. (Criterion G.1)

Official Values

Criterion A Processes

The Defence National Storage and Distribution Centre (DNSDC) is historically highly significant. As a military storage site it dates from 1915, and the Centre is important for its associations with the development of Australia's military forces prior to and during the First World War and particularly for its direct association with the military build up in the early years of the Second World War. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The place also has an association with early nineteenth century settlement in the Liverpool area.

Attributes

The fabric, form and set out of all of the buildings, including their alignments and orientations to former roads and railways.

Criterion B Rarity

Particularly important are the fifteen timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value.

Attributes

The post and beam warehouses.

Criterion D Characteristic values

The DNSDC contains twenty Second World War post and beam warehouses, many of which, despite being re-clad, are good examples of their type. In addition to the fifteen timber post and beam military warehouses of the nine-bay type are the three composite steel and timber type warehouses, which are also important. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s. Further, the alignment of part of the former military railway system is evidenced by the alignment and siting of some of the buildings and roads at the site.

Attributes

The fabric, form and set out of all of the buildings, including their alignments and orientations to former roads and railways.

Criterion G Social value

The Centre is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site.

Attributes

Not clarified.

Description

HISTORY

The Defence National Storage and Distribution Centre (DNSDC) at Moorebank, which is part of the larger Moorebank Defence site, stands within part of the original 6000 acre property granted to Thomas Moore in 1805-21. The eastern boundary of the DNSDC site follows the alignment of part of Moore's property. Moore was the first magistrate at Liverpool and was an influential figure in the early years of the area. The northern and western boundaries of the DNSDC follow boundaries of the later 1888 Moorebank Farms subdivision.

Meanwhile, military activities began in the Liverpool area early in the 1800s when troops guarded convicts and a military barracks was constructed in the town. Military use of the area increased early in the twentieth century, and training camps were run. Military manoeuvres were held for the inspection of Lord Kitchener in 1910, and a rifle range was developed. The military camp was located partly on the present DNSDC site.

One result of Kitchener's visit was the acquisition by the Commonwealth of large areas of land for military purposes, and in 1913 an area exceeding 16,000 acres and including the DNSDC site was acquired by the federal government for defence purposes. This was the era of compulsory training and permanent military training areas, hence the Commonwealth's need to acquire large areas. Military storage at the DNSDC appears to have begun in 1915.

The Liverpool camp was the main military training centre in NSW during the First World War. All new recruits came here, including Light Horse units. In 1919 two railway sidings were added in order to service the depot, and were part of the larger railway system which linked the various military establishments in the area. One of the sidings was extended during the Second World War.

As mentioned above, military storage began at what is now called the DNSDC in 1915, during the First World War. Called initially the Mobilisation Stores, the place accommodated the 2nd Base Ordnance Depot and 2nd Base Workshops during the Second World War. In 1972 these units became the 21st Supply Battalion and the 2nd Base Workshop Battalion. In 1990 these two joined to become the Moorebank Logistics Group.

During the Second World War there were perhaps 20 timber post and beam buildings erected, as well as other buildings. All these structures reflected the widespread expansion of support facilities during the early part of the war.

From the 1940s to the early 1990s there was little development at the site. Then in the early 1990s the place was upgraded to become the DNSDC. Most of the Second World War buildings were reclad and the original asbestos cement sheeting was removed. A small number of buildings was demolished and replaced with more modern designs. The creation of the DNSDC was an element of the Defence Logistics Redevelopment Project which aimed at rationalising Defence warehousing. DNSDC is the main warehouse for all three of the Australian armed services, and there are maintenance sections as well.

DESCRIPTION

The place is mainly made up of a number of large storage sheds, many dating from the early 1940s, which have been variously extended and refurbished as part of the early 1990s work. There is also a number of smaller ancillary, administration and workshop buildings. Near the main entrance stands the main headquarters and administration building for the DNSDC.

Twenty Second World War timber post and beam buildings of various configurations appear to remain, all having been reclad. Of these, 15 (building numbers 6, 10-11, 33-35, 39-40, 44-46, 48, 72-73 and 75) are of timber post and beam construction, with 9 internal bays (the widest type built). With their original timber structure intact, they have been reclad with modern profiled steel sheeting, have concrete floors, and sprinklers and smoke management screens. Three of the 20 buildings (7, 9, 80) are composite timber and steel warehouse structures, with three bays of timber post and beam construction on either side of a central raised bay, steel-framed to support a gantry crane. The other two buildings are the QM Store (13) five bays wide, and the Carpentry Workshop (37) three bays wide and extended.

The timber used in the 20 post and beam buildings is Oregon. It is understood that the buildings were prefabricated in the United States and shipped to Australia on US Liberty Ships.

Post and beam stores buildings were built during the Second World War in numbers of military establishments in eastern Australia, and particularly in south-eastern Australia. They played a major military warehousing role at the time. In 1995 a number remained in use, including at Bogan Gate, Regents Park and Wallangarra in NSW and at Broadmeadows and Bandiana Victoria. However, these types of buildings are now fairly rare, for by 2002 the only other known site with similar Second World War timber store buildings still in Defence ownership, was at Bandiana. Hence the examples at DNSDC have rarity value as well as being good examples of the type (particularly in the case of the nine-bay types).

The alignment of part of the former Liverpool - Anzac Rifle Range - Holsworthy military railway line and sidings can still be seen via the alignment and siting of some of the buildings and roads at the site. The Ordnance Depot railway siding constructed in 1919 and extended

during the Second World War has been mainly demolished.

History Not Available

Condition and Integrity

The timber post and beam buildings at DNSDC were reclad in the early 1990s. The DNSDC is in the course of being disposed of by Defence. (October 2002)

Location

About 83ha, at Moorebank, being an area bounded by a line commencing at the intersection of Moorebank Avenue with an unnamed road at approximate AMG point 307820mE 6240100mN, then easterly and northerly via the unnamed road to its intersection with Anzac Road and Greenhills Avenue, then westerly via Anzac Road to its intersection with Moorebank Avenue, then southerly via Moorebank Avenue to the point of commencement.

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Nolan, Gregory, Australian Timber Buildings of the Second World War.
www.oak.arch.utas.edu.au/projects/retro/war/war.html 2002.

MAY 2001

HERITAGE ASSESSMENT



DEPARTMENT OF DEFENCE SYDNEY PROPERTY DISPOSALS UNIT

DEFENCE NATIONAL STORAGE DISTRIBUTION CENTRE (DNSDC) Moorebank Defence site, Moorebank

GRAHAM BROOKS AND ASSOCIATES PTY LTD
ARCHITECTS AND HERITAGE CONSULTANTS

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1.0 INTRODUCTION

This report has been extracted from a Heritage Assessment of the Moorebank Defence site at Moorebank, currently being prepared by Graham Brooks and Associates Pty. Ltd., on behalf of the Department of Defence Sydney Property Disposal Unit, as part of the master planning exercise to secure the rezoning of the overall site.

The purpose of this report is to complement other environmental and planning assessments prepared that support the rezoning of the DNSDC site from solely military use to various employment generating uses.

1.1 SITE IDENTIFICATION

The Defence National Supply and Distribution Centre (DNSDC) forms part of the Moorebank Defence site, located approximately 2 km southeast of Liverpool, on the eastern bank of the Georges River. The Moorebank Defence site also includes the School of Military Engineering (SME) and Base Administrative Support Centre (BASC) located on the western side of Moorebank Avenue, the former Ammunitions Depot located on the southern side of Anzac Road, and two areas of natural scrub and bushland.

1.2 HERITAGE LISTING STATUS

The DNSDC site within the Moorebank Defence site has not been included in the Cubbitch Barta National Estate area listed in the *Register of the National Estate* by the Australian Heritage Commission. It has not been listed on the State Heritage Register by the NSW Heritage Council or identified as a heritage item on the Liverpool LEP 1997.

The DNSDC site within the Moorebank Defence site is located within the Holsworthy Landscape Conservation Area, which has been Classified by the *National Trust of Australia*. Although the listing does not specifically identify the subject area, the boundary definition includes the subject area within the 20,310 hectares of land covered by the listing. Listing on the Register of the National Trust carries with it no statutory implications.



Fig. 1.1 LOCATION PLAN

2.0 HISTORIC OUTLINE

The association of the military with the Liverpool district extends back to the early 1800s when military troops were stationed at Liverpool to protect the town and oversee convict work gangs, and military barracks were constructed at the corner of George and Moore Streets.

During the early 1900s the use of the Liverpool area for military purposes steadily increased, with several training camps held in the area to the north of the DNSDC site. By 1907 a military camp had been established across the Georges River from the Liverpool township, with a rifle range further to the south, as shown on a 1915 plan of the Liverpool Manoeuvre area.

In January 1910 the Liverpool camp held manoeuvres for the inspection of Lord Kitchener, who was to advise the government upon the best means of developing the defence forces of the country. The camp was conducted at a time when the Government was reassessing its strategic approach to the military forces of Australia, from solely an expeditionary force to fight Britain's imperial wars, to incorporate a defence force for Australia within the Pacific region.

Although the exact extent of the Liverpool Military camp at this time is unknown, it would appear from the following description of the area used for the manoeuvres contained within the *Daily Telegraph*, that the land presently occupied by DNSDC formed part of this early military camp.

*"The camp was pitched upon the paddocks to the left of the railway station, on the ground that has been similarly occupied in recent years and which is nearly all included in the military manoeuvre area which the Commonwealth Government is endeavouring to secure ... the training ground embraces a stretch of country extending from Liverpool, on the southern line, across Heathcote on the Illawarra system, and it provides not only very fair opportunities for moving large bodies of troops in tactical exercises, but also has within its limits well equipped ranges for artillery and infantry shell and ball practice."*¹

The result of Kitchener's visit was the acquisition of large areas of land by the Government for the establishment of permanent camps for military training and services. In 1912, 883 acres of land were acquired for the establishment of a Remount Depot and Veterinary Hospital for horses used for transport, and in March 1913 a further 16,868 acres of land in the Liverpool area, including the DNSDC site, were acquired by the Government for Defence purposes.² The present DNSDC site would appear to have been used for storage for military purposes since 1915, as illustrated on a 1915 plan of the Liverpool Manoeuvre area.

¹ *Daily Telegraph*, 7 January 1910, p.7.

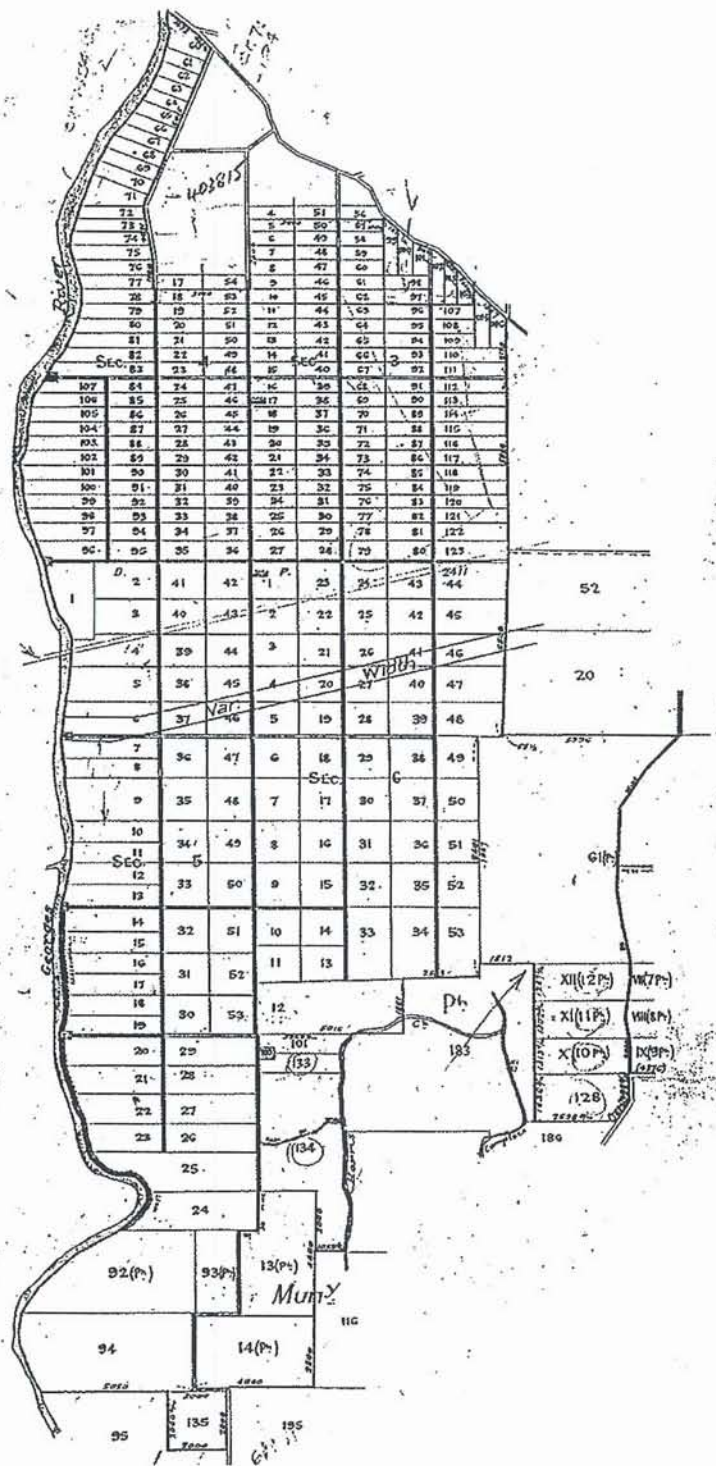
² *Commonwealth of Australia Gazette* No. 16, 7 March 1913.

The following plan of the Moorebank area c.1912 provides an illustration of the type of development located at the study area at this time. The site of the DNSDC at the time appears to remain as dense gum ironbark and wattle scrub, as was the majority of the land to the east of the Georges River, including the present site of the DNSDC.

A number of roads are shown to exist at this time, generally corresponding to original roads or boundaries of the Moorebank Farms Subdivision, including Moorebank, Greenhills and Centenary Avenues running north-south, and Church, Henley and Harrow Roads running east-west. All but Moorebank Avenue and a section of Greenhills Avenue to the north of Anzac Road, have since been obliterated.



Fig. 2.1 PLAN OF THE MOOREBANK AREA INCLUDING THE STUDY AREA c.1912.
Source: Liverpool Regional Museum



Source: Certificate of Title Vol. 2788 Fol. 87, Land Titles Office

The following plan of the Liverpool Manoeuvre area dated c.1915 shows the Liverpool camp. At the present DNSDC site are noted "Mobilisation Store" on the eastern side of Moorebank Avenue.

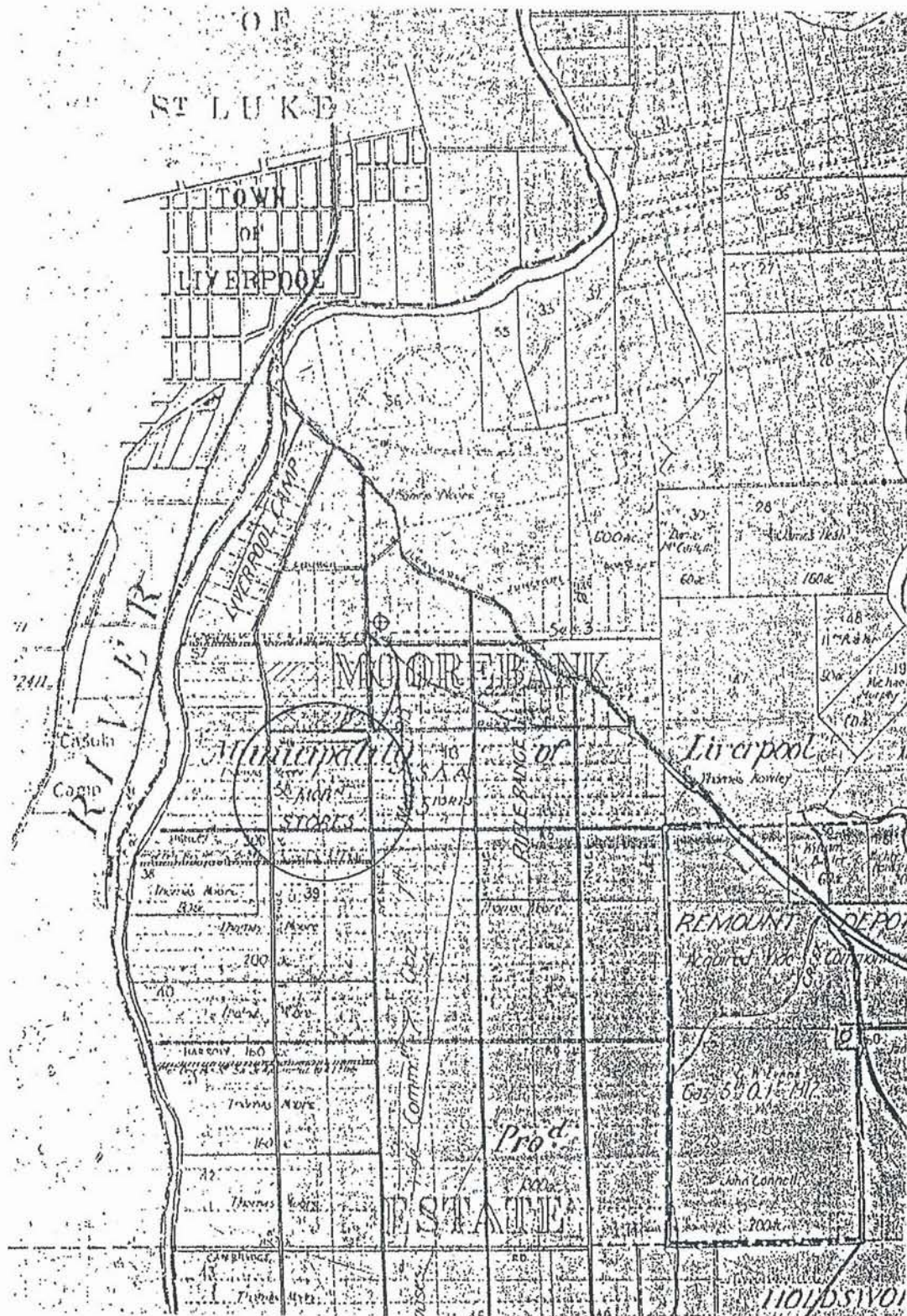


Fig. 2.3 PLAN OF THE LIVERPOOL MANOEUVRE AREA c.1915
Source: Liverpool Regional Museum

During the First World War the Liverpool camp was the main training centre in NSW for all new recruits, including Light Horse regiments. New recruits were encamped in long lines of canvas bell tents on the eastern bank of the Georges River, and the camp became a "city of canvas". Located at the Liverpool camp were the supply units, the Light Horse, the infantry reinforcement contingents and the recruit training depot. Units that trained at the Liverpool camp during the First World War included the Engineer and Field Mining companies, the field hospital, infantry and reinforcement units, artillery and light horse units.

In 1919 two sidings to service the depot were added, off the Liverpool – Anzac Rifle Range – Holsworthy railway line. This railway line had been constructed in 1917 for the Commonwealth Department of Railways and the Department of Defence, in order to link all of the military installations on the eastern side of the Georges River, including the Liverpool Camp, Anzac Rifle Range, the Remount Depot and Holsworthy Camp. The first siding extended through the centre of the site, and was known as the Moorebank Depot siding, and the second, known as the Ordnance Store siding, extended along the northern boundary. The Ordnance Store siding was extended during the Second World War in order to handle the subsequent increase in traffic.

Storage for military purposes would appear to have been accommodated at the present DNSDC site since 1915, at the time of the initial establishment of the Liverpool camp during the First World War. Initially known as the Mobilisation stores, the area accommodated the 2nd Base Ordnance Depot and 2nd Base Workshops during World War II, which became the 21st Supply Battalion and 2nd Base Workshop Battalion in 1972. In 1990 these two groups amalgamated to become the Moorebank Logistics Group.³

Although the type and extent of storage facilities offered at the site during World War II remains unknown, a Department of Defence study of timber buildings constructed between 1939-1945 identified at least fifteen timber post and beam buildings typical of this period at the site.⁴ The buildings identified were each of nine bays in width. An inspection of the site confirmed this report, and identified a further three composite timber and steel workshop buildings, a smaller QM store and carpentry workshop, which all exhibit similar timber post and beam construction. An aerial photograph dated 1951 would indicate that twenty of these typical nine bay post and beam buildings were constructed at the site, most likely as part of the widespread expansion of Army support facilities in the early years of the Second World War.

The various plans and aerial photographs of the DNSDC site would indicate that the site remained largely unaltered between the early 1940s and the early 1990s, when the site was upgraded to accommodate the DNSDC. At this time the majority of the World War II timber post and beam buildings and composite steel and timber buildings were re-clad, to remove the original fibro asbestos sheeting. Five of the original 20 nine bay wide post and beam buildings located at the south western corner of the site, were demolished at this time, and replaced with larger modern steel framed warehouse buildings. Following the completion of the DNSDC, the site comprised of a mixture of large and small storage buildings, and smaller ancillary buildings for administration, workshops and amenities.

³ *Minutes of Evidence, Defence Logistics Redevelopment Project, Defence National Storage and Distribution Centre.* Parliament of the Commonwealth of Australia, 1992.

⁴ Department of Architecture, University of Tasmania. *Department of Defence Timber Buildings 1939-1945*, March 1995, pp. 92-93.

The DNSDC at Moorebank was established as part of the Defence Logistics Redevelopment Project, to centralise and rationalise its warehousing facilities. The DNSDC resulted from the Department of Defence's requirement to improve the effectiveness and efficiency of Defence supply services. Existing warehousing arrangements in and around Sydney did not provide Defence with the most cost effective logistic support. Slow moving and bulky stock is still warehoused at Leichhardt, Regents Park, Dubbo and Bathurst.

DNSDC is the central warehouse for all three of Australia's armed services, including everything from electronic gadgets, army rations, food supplies, naval guns, aircraft wings and bodies. There are also maintenance sections dealing with automotive engineering, electronics, vehicle repairs, small arms and heavy precision engineering.

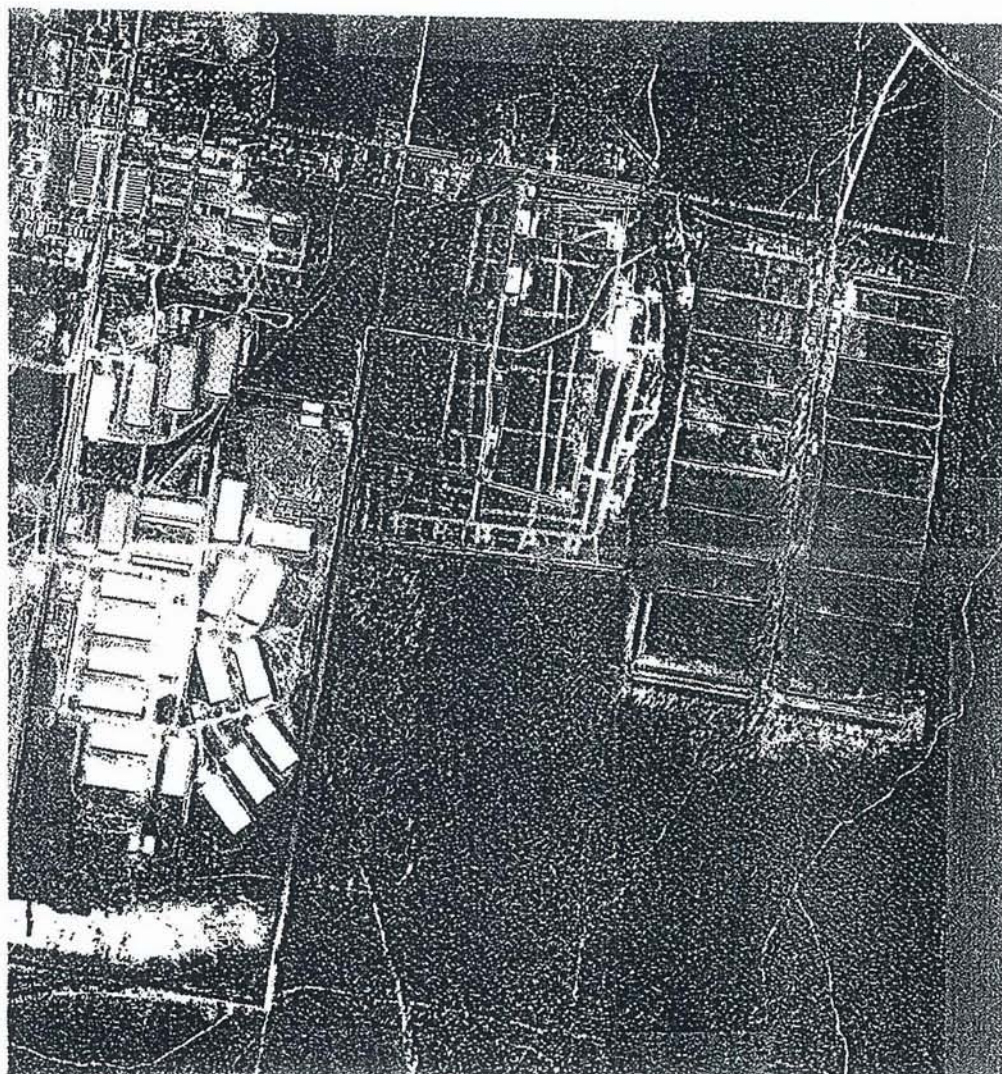


Fig. 2.4 AERIAL PHOTOGRAPH SHOWING THE DNSDC SITE IN 1951
Source: Department of Lands NSW

3.0 DESCRIPTION

The DNSDC study area is a rectangular shaped parcel of land of approximately 84 hectares, which extends towards Anzac Road in the north, and is bound by Moorebank Avenue in the west, the former Ammunitions Depot in the east, and an area of natural scrub and bushland in the south and east.

Access to the site is provided at a number of locations along Moorebank Avenue, however the main entrance is approximately midway along Moorebank Avenue.

The site is predominantly made up of a number of large storage sheds, many of which would appear to be those dating from the early 1940s, although which have been extended and refurbished in the early 1990s, as well as a number of smaller ancillary, administration and workshop buildings. Adjacent to the main entrance is the main headquarters and administration building of the DNSDC.

Twenty World War II timber post and beam buildings of varying sizes and types would appear to remain at the site, however all have been reclad as part of the early 1990s refurbishment. Fifteen of these buildings (Nos 6, 10-11, 33-35, 39-40, 44-46, 48, 72-73 and 75) are timber post and beam, with nine internal bays, and retain the original timber structure essentially intact, and have been reclad with modern profiled steel sheeting, new concrete floors laid, and sprinklers and smoke management screens installed. Three of these buildings (buildings 7, 9 and 80) are composite timber and steel warehouse buildings, which have three bays of timber post and beam construction on either side of a central raised bay, which is steel framed to support an overhead gantry crane. The remaining two buildings are the smaller QM Store (building 13), which has a similar timber post and beam construction of five bays in width, and the Carpentry Workshop (building 37), which is three bays wide, and which has been extended along both its length and width.

A railway siding servicing the Ordnance Depot, constructed in 1919 and extended during the Second World War, has been largely demolished, although there remains evidence in the landscape of its original alignment and the location of early buildings.



Fig. 3.1 BUILDING 9 AT THE CENTRE OF THE DNSDC SITE, A TYPICAL WORLD WAR II COMPOSITE TIMBER AND STEEL WAREHOUSE BUILDING, OF WHICH THERE ARE THREE AT THE SITE, AND WHICH HAS BEEN REFURBISHED DURING THE EARLY 1990s.



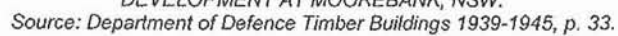
Fig. 3.2 INTERIOR OF BUILDING 9, A WORLD WAR II COMPOSITE TIMBER AND STEEL FRAMED STORE BUILDING



Fig. 3.3 TYPICAL INTERIOR OF A WORLD WAR II TIMBER POST AND BEAM STORE BUILDING AT DNSDC MOOREBANK



Fig. 3.4 DETAIL OF THE TYPICAL TIMBER STRUCTURE TO WORLD WAR II POST AND BEAM STORE BUILDINGS AT DNSDC, MOOREBANK



3.1 TIMBER POST AND BEAM STORES COMPARATIVE ANALYSIS

There were a considerable number of standard designs for post and beam stores constructed during World War II, with the number of bays in transverse section ranging from two bays, about 10m wide, to nine bays, about 48m wide. Bay widths also varied from 4.15m, to 5.08m and 6.1m wide. A typical structure comprised of timber posts fixed to a concrete floor, joined to the top of each of the posts in each row by longitudinal tie beams, and inclined transverse rafters jointed over each post. Transverse knee braces connected the timber posts to the inclined rafters, while longitudinal ones braced the post to the tie beams, thereby stabilising the overall structure.

Post and beam stores buildings were erected during World War II for all services along the eastern seaboard, however they appear to be concentrated in south eastern Australia. They provided the mainstay of general ordinance storage, and in 1995 a significant number remained in use, including at Bogan Gate and Regents Park, NSW for the RAAF, Wallangarra, NSW for the Army, and at Broadmeadows and Bandiana, Victoria for the Army.⁵

Most of the timber post and beam store buildings located at the DNSDC site are typical of the nine bay type constructed during World War II. The early aerial photographs indicate that twenty such store buildings were constructed at the site.

3.1.1 Comparative Analysis, Bandiana, Victoria

Military use of Bandiana, Victoria arose following Japan's entry into World War II at the end of 1941. At that time, the perceived threat of an air attack on Melbourne and Sydney, resulted in a requirement for an inland storage facility, which would be safe from such an attack, while remaining close to existing transport facilities. One of the determining factors in the selection of Bandiana was its closeness to the major railway systems of both NSW and Victoria.

The main emphasis of the Bandiana facilities during the war years was the storage and processing of technical and motor transport stores, and the early construction at the site generally mirrored that which was occurring elsewhere.

Of the wartime construction at Bandiana, the storehouses and workshops within the North Bandiana precinct date from 1942, and are historically important. These buildings remain relatively unchanged, and were constructed in five rows, including three workshop buildings of timber framed construction, four storehouses with internal rail access, nine storehouses constructed on concrete plinths, and three transit stores. They have a timber post and beam construction, with a concrete floor, timber framed corrugated iron clad walls and fibro roofs. The timber used for the construction is Mountain Ash, which was sourced in the vicinity of Myrtleford, Victoria. The buildings at North Bandiana are the most comparable to the store buildings at DNSDC Moorebank.

The three workshop buildings (buildings 96, 100 and 105) are of varying timber framed construction, and have each been refurbished to an extent, including new external sheeting and internal partitioning.

⁵ Department of Architecture, University of Tasmania. *Department of Defence Timber Buildings 1939-1945*, March 1995, pp. 92-93.

The four storehouses constructed with internal rail access (buildings 64, 82 and 89-90) are of composite timber and steel construction. The buildings comprise a lower wing of five bays of timber post and beam construction, and a raised wing of steel framed construction, which straddles the railway line.

The three transit stores (buildings 68, 71 and 74) have a composite timber and steel framed construction, which is similar to that found at DNSDC. The gantry crane extends through the building on one side.

The eight storehouses constructed on concrete plinths (buildings 65, 69, 72, 77 and 79) most resemble the stores buildings at DNSDC, and are located in two rows through the site. These have a similar timber post and beam construction, although are slightly smaller, being only five bays wide.

The storehouse and workshop buildings at North Bandiana are located in their original wartime configuration, and retain evidence of the important railway connections through the site. The North Bandiana site has significance as evidence of strategic logistics support planning and massive expansion in military operations during World War II.

At South Bandiana four comparative timber post and beam stores buildings have been identified, although these are scattered through the site, and don't form a cohesive precinct. The present Army Museum most resembles the stores buildings at DNSDC. This building has a similar timber post and beam construction of nine bays in width, although has been refurbished with modern steel sheeting. The three remaining timber post and beam store buildings are slightly smaller, being only five bays wide.

Similar storehouse and workshop buildings were also constructed at East and West Bandiana.



Fig. 3.6 INTERIOR WORKSHOP 105, REFURBISHED FOR USE BY THE ARMY RESERVES, SHOWING TIMBER CONSTRUCTION

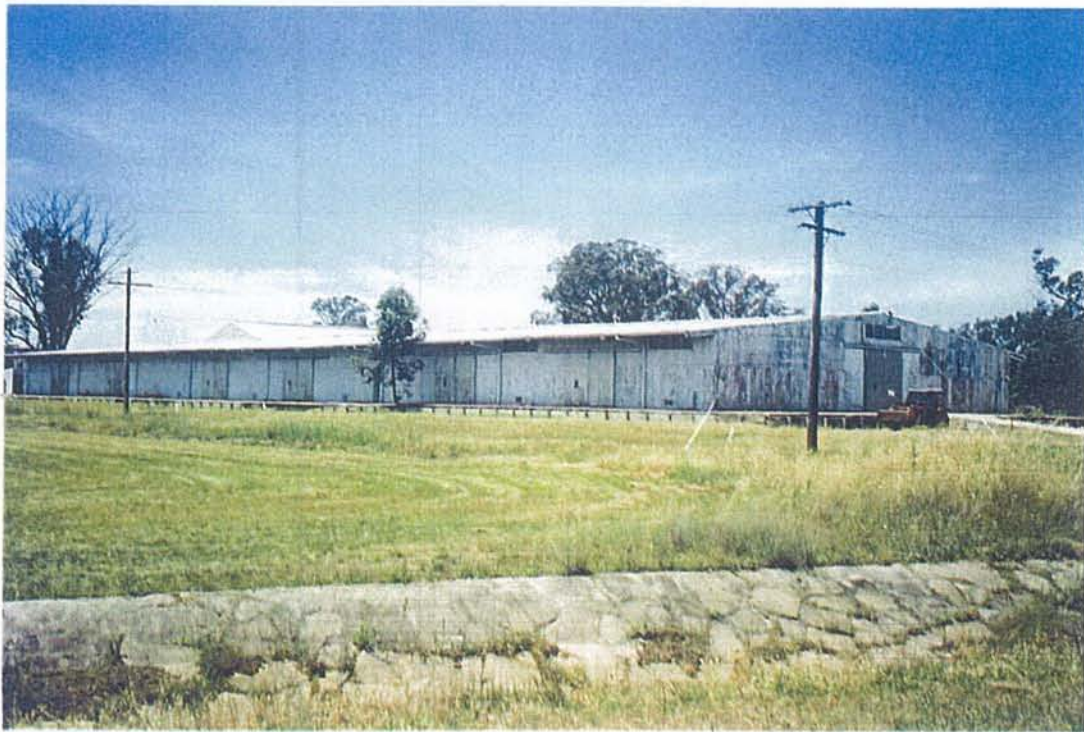


Fig. 3.7 TYPICAL EXAMPLE OF THE EIGHT TIMBER POST AND BEAM STOREHOUSES CONSTRUCTED ON CONCRETE PLINTHS, LOCATED AT NORTH BANDIANA



Fig. 3.8 INTERIOR OF A TYPICAL TIMBER POSTS AND BEAM STOREHOUSE AT NORTH BANDIANA, SHOWING SIMILAR TIMBER CONSTRUCTION TO THAT FOUND AT DN3DC



Fig. 3.9 TYPICAL EXAMPLE OF THE FOUR STOREHOUSES AT NORTH BANDIANA, WHICH COMPRISE OF A LOWER TIMBER WING, AND A RAISED STEEL FRAMED WING WHICH STRADDLES THE RAILWAY LINE



Fig. 3.10 ONE OF THE THREE TRANSIT STORES AT NORTH BANDIANA, WHICH HAVE A COMPOSITE TIMBER AND STEEL CONSTRUCTION SIMILAR TO THAT FOUND AT DNSDC

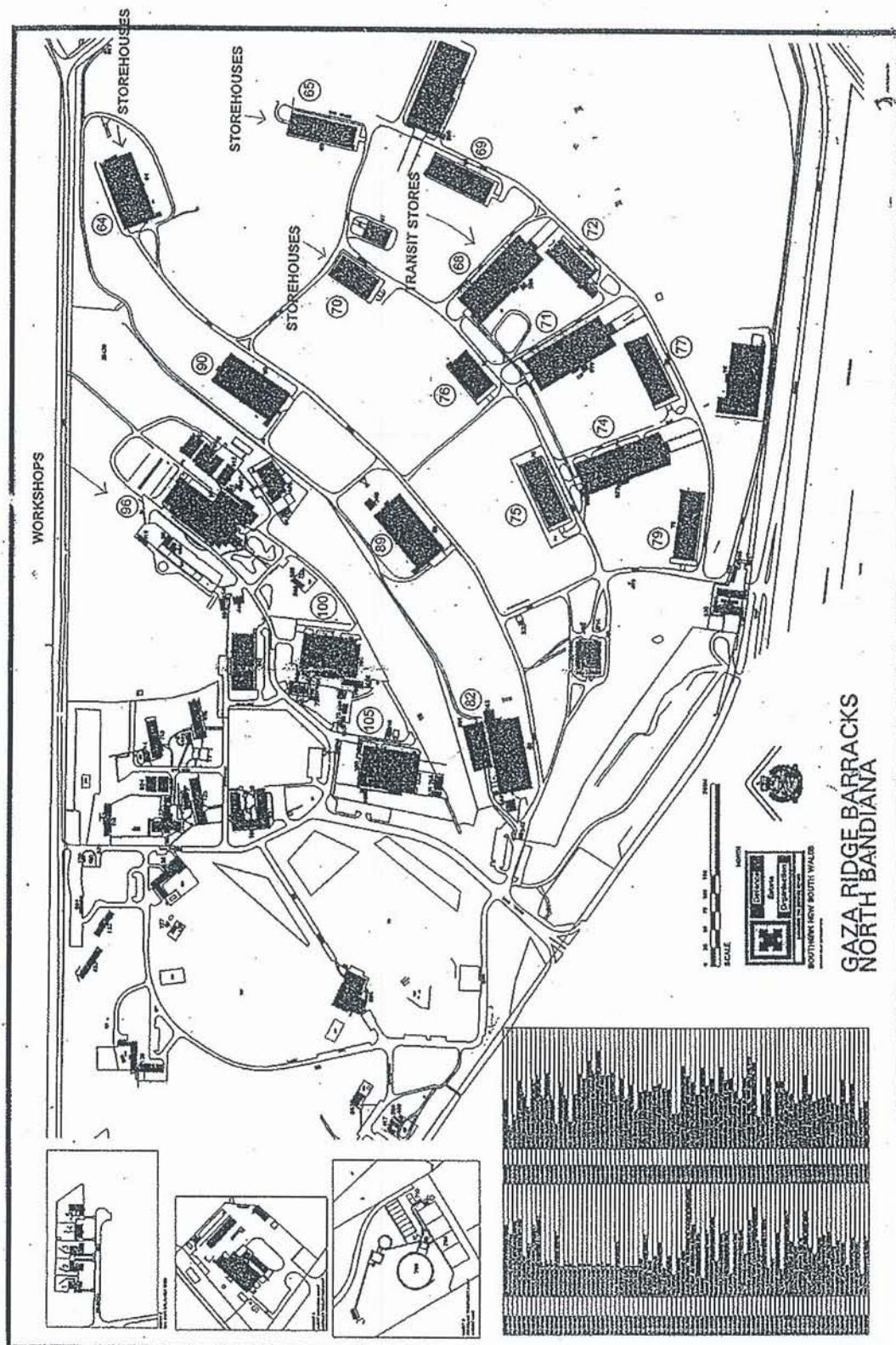


Fig. 3.11 PLAN OF NORTH BANDIANA, SHOWING ARRANGEMENT OF STOREHOUSES AND WORKSHOPS

4.0 ASSESSMENT OF SIGNIFICANCE

'Heritage' significance, or 'cultural' significance are terms used to describe an item's value or importance to our society, and is defined in *The Burra Charter*, published by Australia ICOMOS (Article 1.2) as:

"aesthetic, historical, scientific or social for past, present and future generations".

The NSW Heritage Manual has adopted these four criteria as the basis for an assessment of the heritage significance of an item. The Manual also establishes evaluative criteria, divided into two categories, being:

- Nature of significance
- Comparative significance

4.1 EVOLUTION OR ASSOCIATION (HISTORIC)

The DNSDC site within the Moorebank Defence site has local historic significance as a part of 6,000 acres originally granted to Thomas Moore between 1805-1821, who as first magistrate of Liverpool, was influential in the early settlement and development of the town. The site has historic significance as forming part of the Moorebank Farms subdivision offered for sale in 1888. As such, the site has historic significance as an example of the state theme of *Land Tenure*, established by the NSW Heritage Office.

The DNSDC site has historic significance at National Level as forming part of the Liverpool military camp, which held manoeuvres in January 1910 for the inspection of Lord Kitchener. The camp is significant for its historic role in the development of the military forces in Australia, at a time when the Government was reassessing its strategic approach. The camp highlighted the need for compulsory training and permanent military training areas, which resulted in the acquisition by the Government of extensive areas of land, which were permanently devoted to military training and services.

The DNSDC site has historic significance as part of the extensive land area in the Liverpool district formally acquired by the Government for Defence purposes in March 1913. The site is representative of the state theme of *Defence*, established by the NSW Heritage Office.

The DNSDC site has historic significance for its continued occupation for Defence Logistics support purposes since 1915, and as evidence of the continued military organisation to sustain operations and defence capability into the long term.

The collection of timber post and beam store buildings at the DNSDC site have significance as rare and representative examples of this type of store building constructed during World War II for military storage purposes. These timber store buildings have historic significance for their association with the United States military, who were influential in the use of timber construction during WWII. The store buildings were reputedly prefabricated in the US, and imported into Australia on US "Liberty" ships.

The DNSDC site has historic significance as part of the massive expansion of military facilities which occurred during World War II. Similar facilities were erected along the eastern seaboard, although concentrated in south eastern Australia, and provided the mainstay of general ordinance storage during World War II.

4.2 CREATIVE OR TECHNICAL ACCOMPLISHMENT (AESTHETIC)

The DNSDC site as a collection of large timber store buildings, does not exhibit any aesthetic significance.

The site does exhibit some aesthetic significance related to its cultural plantings, in particular along Moorebank Avenue.

4.3 COMMUNITY ESTEEM (SOCIAL)

The DNSDC site has social significance for the extensive community of Defence personnel, which work at the site, and for the immediate community of Liverpool and the broader community of Sydney, as the location of Defence operations since 1915.

4.4 RESEARCH POTENTIAL (SCIENTIFIC)

The DNSDC site has some scientific significance for its ability to show evidence of the early boundaries and alignments of the original land grants in the area, the 1888 Moorebank Farms subdivision, and part of the Liverpool – Anzac Rifle Range – Holsworthy military railway line.

The DNSDC site has some archaeological significance for its potential to yield information regarding the early use of the site, in the alignment of the 1919 railway line and siding which formerly serviced the site.

The collection of timber post and beam store buildings located at the DNSDC site have significance for their innovative construction method, as rare and representative examples of timber post and beam store buildings constructed during World War II. This technical significance is further discussed below.

4.5 OVERALL SIGNIFICANCE OF THE WORLD WAR II STANDARD TIMBER BUILDINGS, AS PART OF A NATIONWIDE GROUP

In order to more fully understand the broader context of the significance of the various timber post and beam stores buildings at the DNSDC site, the following summary of significance has been included, drawn from the report *Department of Defence Timber Buildings 1939-1945*.⁶

"These buildings are culturally significant as they demonstrate the versatility and self-reliance of the Australian Government and people in a time of national emergency. Under direct military threat, the nation embarked on total mobilisation in its own defence and as it did so, reorganised itself to

⁶ Department of Architecture, University of Tasmania. *Department of Defence Timber Buildings 1939-1945 Final Report*, 1995, pp. 61-63.

make the most efficient use of the resources at hand. While the labour force mobilised and the organisations instigated were largely transient, the facilities constructed and used during the conflict were not. They remain a national asset and a testament to the nation's reaction. That these facilities were often constructed simply, ruggedly and in haste merely demonstrates the extent of emergency.

They are culturally significant due to the large public association of these buildings with times of personal and national change and stress.

The buildings are historically significant because the forms and location of the structures depict the strategic reality facing Australia at the time of their construction. Prior to December 1941 development was urgent but considered, and timber construction was largely limited to the scale of building constructed before the war, that is, the timber hut building. These buildings were generally temporary structures for the training of troops. In early 1942, construction in timber was hectic and experimental, concentrating on south eastern Australia. Large complexes were constructed to provide stores, airfields and war industry plant. By 1943, experimentation lessened but the pace of building was maintained. Facilities were developed to bolster Australia's defence and to provide forward supply bases for battles fought in the Pacific Islands. By 1944, the nation served as a storage and staging base for advances throughout the Pacific, and the buildings of that time concentrated around the major posts of Brisbane, Sydney, and to a lesser extent Melbourne.

The buildings have a political and technical significance as they show how standing design preferences and practices were overthrown as part of the national reorganisation. Technologies that had previously had little impact in Australia were used extensively, while technologies introduced by the USA military were embraced. Unseasoned local hardwood, a material that had previously been regarded as unsuitable for large buildings achieved primacy. The technical achievements of this period for timber construction cannot be overstated. The longest span and most widespread timber structures in Australian history were constructed in this period. Almost every specie of Australian timber was placed in extreme field test.

This significance has a further facet as the timber construction forms and technologies used throughout the war did not survive it. With demobilisation the pre-war preferences for steel construction re-emerged and timber construction for structures larger than houses did not recover its national popularity again until the 1960s.

Aesthetically, the truss and other construction forms produced throughout the war are unique. They advanced the aesthetic which had lingered as a legacy from the king and queen posts forms used in Australian buildings with the mortice and tenon construction of the later 1900s, and lack the nostalgia invoked in the 1960s and 1970s for the farmhouse. They express themselves in true and clean engineering layouts as was essential in a time of emergency.

These buildings, located throughout Australia, therefore have a significant heritage value as a group that should be preserved, recognising the influences that determined the form and construction distribution of the group."

5.0 HERITAGE ISSUES

5.1 FEATURES WHICH CONTRIBUTE TO SIGNIFICANCE

The various elements of the DNSDC site have been carefully assessed to determine their relative level of contribution to the significance of the DNSDC site, as either of high or low contribution. Grading attempts to identify the level of contribution which individual elements make to the overall significance of the DNSDC site.

The following elements are considered to make a **high contribution** to the significance of the DNSDC site.

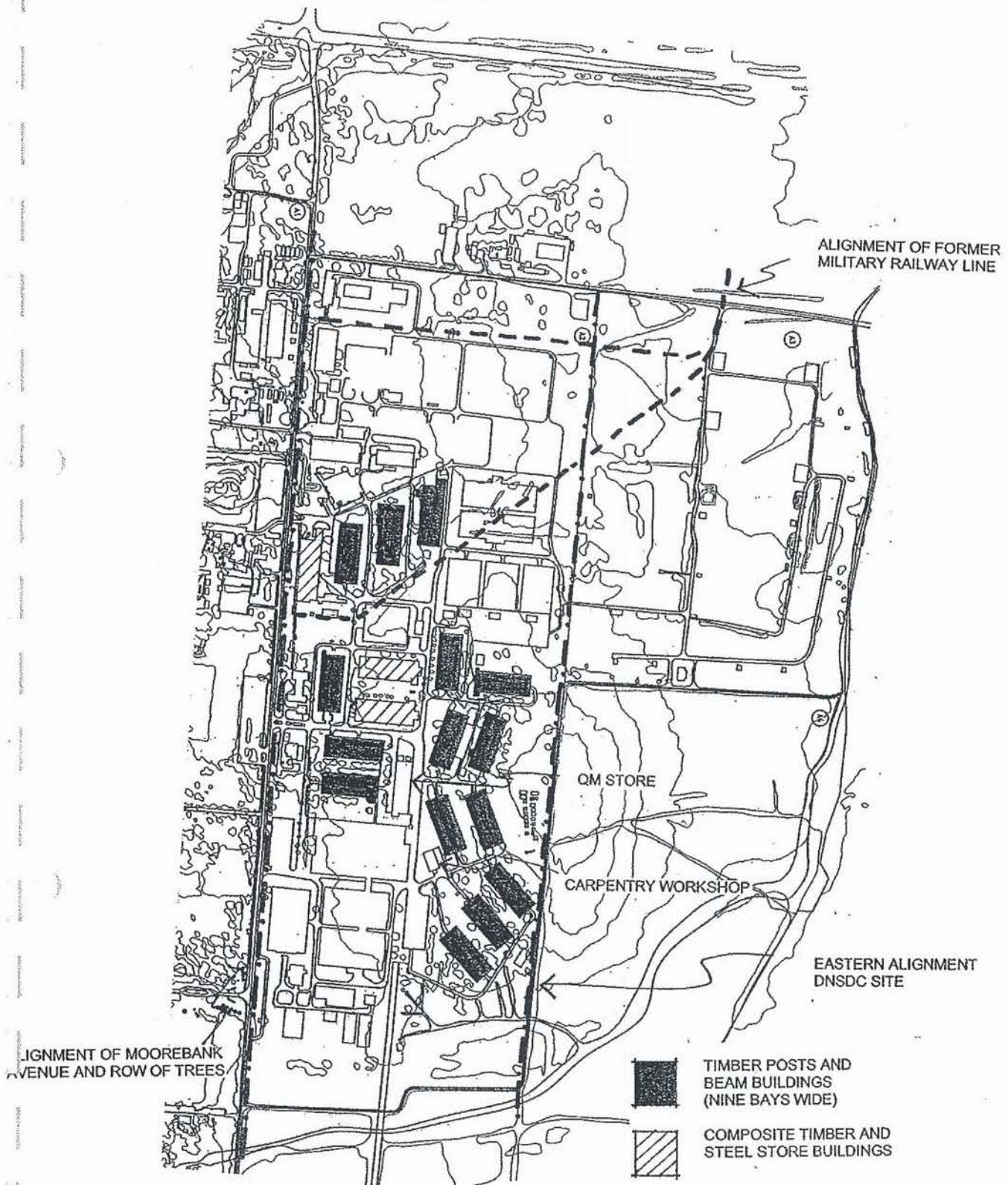
- The eastern boundary of the DNSDC site, which follows the alignment of Thomas Moore's grants along the Georges River, dating from the early 1800s.
- Moorebank Road and Anzac Road, which follow the alignments of the 1888 Moorebank Farms subdivision.
- The remnant evidence of the alignment of part of the former Liverpool – Anzac Rifle Range – Holsworthy military railway line and sidings, which can be seen within the alignment and placement of buildings and roads at the site.
- The collection of remaining timber post and beam buildings, which date from World War II, and which retain the internal timber structure essentially intact, as representative examples of such timber store buildings constructed during the second World War, and which are becoming increasingly rare in NSW.
- Significant avenue plantings along Moorebank Avenue.

The following elements are considered to make a **low contribution** to the significance of the DNSDC site.

- The remaining buildings of the DNSDC site, for their contribution to the ongoing use of the DNSDC site for military storage purposes since 1915.

5.2 COMPARATIVE SIGNIFICANCE, BANDIANA, VICTORIA

The comparative timber post and beam buildings at North Bandiana, identified in Section 3.1.1 Comparative Analysis, generally retain a high degree of integrity in their original construction, including the original timber structure, external cladding and concrete plinths. Furthermore, their original wartime configuration, being located in five rows connected by a railway system, is also readily comprehensible. As such, it is considered that these buildings at North Bandiana are more worthy of conservation than those at the DNSDC site.



FEATURES WHICH CONTRIBUTE TO SIGNIFICANCE

DEFENCE NATIONAL STORAGE AND DISTRIBUTION CENTRE (Not to Scale)

6.0 RECOMMENDATIONS

The following recommendations have been formulated given the assessed significance of the DNSDC site, and the level of contribution of the various elements of the site to that significance.

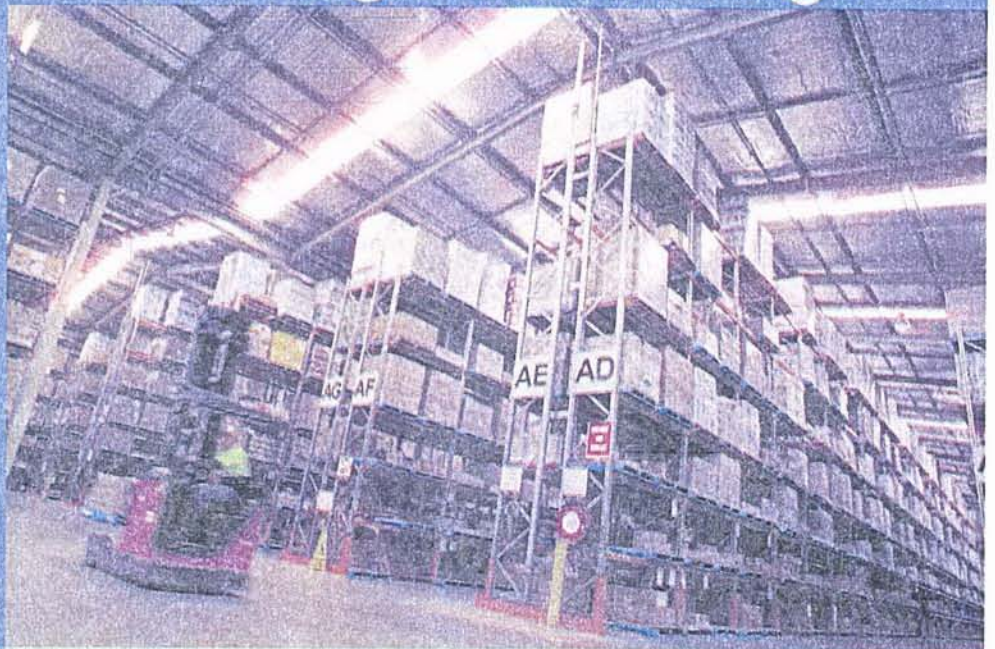
- Given their increasing rarity, the preferred conservation option for the collection of World War II timber post and beam stores buildings is for the retention of one or more of the buildings as a representative example, for continued use by the Department of Defence, or adaptive re-use by others, provided that a viable re-use of the buildings can be identified.
- Should one or more of the stores buildings be retained for conservation purposes, any adaptive re-use of them should be compatible to their architectural character and significance, and have minimal impact on their original timber structure. As such, alterations which require change or removal of the original timber structure, with the exception of maintenance, are not considered acceptable.
- Should the re-use of the World War II buildings not be considered prudent or feasible, then demolition of all of the buildings would be acceptable, given the preparation of a photographic recording and measured drawing survey of the site.

A photographic recording and measured drawing survey of the site were completed in March 2001. The photographic recording covered the whole collection, with a focus on the four different World War II timber building types, while the measured drawing survey took into account the two main types of World War II buildings located at the site. These documents are considered sufficient as an archival record the site, and satisfy the requirements of its significance.

- The cultural landscape of the DNSDC site, including the eastern boundary of the site, alignments of Moorebank and Anzac Roads, and the remnant evidence of the alignment of the former Liverpool – Anzac Rifle Range – Holsworthy military railway line and sidings, should be expressed in future proposals for redevelopment.
- The avenue plantings along Moorebank Avenue, and other mature trees throughout the site, should be retained and conserved.

artefact

Non-Indigenous Heritage



SIMTA

SYDNEY INTERMODAL TERMINAL ALLIANCE

Transitional Part 3A Concept Plan Application

21/11/2011

Executive Summary

Artefact Heritage, on behalf of the Hyder Consulting and the Sydney Intermodal Terminal Alliance (SIMTA) (a consortium of Qube Logistics and QR National), has undertaken an assessment of non-Indigenous heritage for the site of a proposed intermodal terminal facility and rail corridor at Moorebank, New South Wales.

The proposed development is an intermodal terminal facility, which will be linked to the Southern Sydney Freight Line and will provide container freight distribution and warehousing facilities. The proposal is a staged development, with the different stages as follows:

- Stage 1 – Construction of the intermodal terminal and rail link.
- Stage 2 – Construction of warehouses and distribution facilities.
- Stage 3 – Extension of the intermodal terminal and completion of warehouses and distribution facilities.

A Concept Plan approval is being sought under the transitional provisions relating to Part 3A assessments under the Environmental Planning and Assessment Act 1979 (EP&A Act). On approval of the Concept Plan for the SIMTA proposal, applications for development stages of the SIMTA proposal will be submitted to the NSW Department of Planning and Infrastructure as State significant development (SSD) under Part 4 of the EP&A Act, or as otherwise stipulated in the Concept Plan approval. This report provides an overall non-Indigenous heritage assessment, for the proposal as a whole, to support the application for Concept Plan approval under Part 3A of the Environmental Planning and Assessment Act 1979.

The SIMTA site, approximately 83 hectares in area, is currently operating as a Defence storage and distribution centre. The SIMTA site is legally identified as Lot 1 in DP1048263 and zoned as General Industrial under Liverpool City Council LEP 2008. The parcels of land to the south and south west that would be utilised for the proposed rail link are referred to as the rail corridor. The proposed rail corridor covers approximately 75 hectares and adjoins the Main Southern Railway to the north and south. The rail line is approximately 3.5 kilometres in length, 20 metres in width (variable width) and includes two connections to the SSFL, one south and one north.

The footprint for the SIMTA proposal includes part of two heritage listed items: the DNSDC site and the School of Military Engineering complex (SME). The DNSDC site, which encompasses the SIMTA site, is currently listed on the Commonwealth Heritage List (CHL) and is protected under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act), while the SME is listed on the Liverpool Local Environmental Plan 2008 and is protected under the *Heritage Act 1977* and the *Environmental Planning and Assessment Act 1979*. A number of heritage listed items are located in the vicinity of the SIMTA proposal footprint. However, only one of these, Glenfield Farm, may be subject to impacts as a result of the proposal. Glenfield Farm is listed on the State Heritage Register (SHR).

DNSDC site

The DNSDC site includes a number of intact store buildings dating to WWII and is significant as a rare surviving example of a WWII military complex. The SIMTA proposal would have a significant impact on the heritage significance of the DNSDC site, which is currently leased by the Australian Defence Force and is therefore listed on the CHL and protected by the EPBC Act 1999. However, the SIMTA site will only be located within a "Commonwealth Area" for as long as Defence leases the site, and once that lease expires or is relinquished, the SIMTA site would no longer be within a "Commonwealth Area" and would need to be removed from the CHL (s341L EPBC Act). It is possible that the site may then be considered for listing on another heritage register, such as the National Heritage List (NHL) or SHR. If either of these listings were to occur prior to the granting of development approval for the SIMTA site, SIMTA would be required to fulfil additional obligations under the relevant heritage legislation.

The ongoing heritage protection measures that will exist once Defence vacates the SIMTA site will entirely depend on:

- The terms of any contractual obligations between SIMTA and Defence that operate at that point in time; and,
- Whether the SIMTA site is subsequently listed on the NHL or SHR and thereby becomes subject to the regulatory requirements of the relevant legislation.

Different legislative requirements will apply to the SIMTA site, depending on when development approval is sought and which form of statutory protection the site is under at that time. However, regardless of the statutory context, the heritage values of the DNSDC site are known to be high and



it is preferable that significant elements of the site are conserved where possible, whether this is through the re-use of the warehouses or the conservation of the most representative samples of the structures.

The SIMTA proposal would have a significant impact on the DNSDC site and its heritage values, although a combination of mitigation measures would minimise this impact where practicable. The SIMTA proposal is likely to involve the demolition and/or removal of all or some of the heritage buildings on the DNSDC site, the construction of new buildings, and landscape modification through the installation of new water, sewerage, trade waste, and power infrastructure. These changes would impact on the heritage significance of the WWII buildings located at the DNSDC site, although it is likely that these impacts would be mitigated by a combination of conservation, adaptive reuse, and relocation of some of the WWII structures.

If buildings are to be demolished, re-use of heritage fabric within an interpretive context would be appropriate and archival recording would be necessary. While some recording was completed in 2001 (Brooks & Associates 2002:28), updates to this record would be required. The historical landscape context of the site should also be taken into account. Elements such as the alignment of the roads and rail line may be preserved, or embedded through conservation or interpretation in the new development design (Brooks & Associates 2002:28).

It is recommended that a mitigation strategy should be developed for the DNSDC site as a whole, once the nature of the SIMTA proposal has been more adequately defined. This strategy may be based on the potential mitigation options outlined in Table 3 and, at a minimum, would involve archival and photographic recording of the entire DNSDC site. At the Project Applications stage, detailed Statements of Heritage Impact (SoHI) should be produced for each stage of the SIMTA proposal, based on the information provided in this report.

It is possible that archaeological remains of former structures exist throughout the site, and these have the potential to be of moderate research significance. Recommendations for mitigation and management measures for areas of archaeological potential would be made within the SoHIs for each stage of the proposal.

SME

Approximately four hectares at the southern end of the main SME complex, and around 16 hectares within the vegetated part of the complex (south of the DNSDC site), is included in the area of the proposed rail corridor that forms part of the SIMTA proposal. However, the proposed rail link itself would only include a narrow strip of land in the vegetated area (approximately 672 metres long and 20 metres wide), before running along the existing East Hills Railway corridor.

Impacts would be limited to a small portion of the SME site, and would not have any impact on the heritage significance of the item.

Glenfield Farm

The SIMTA proposal includes the construction of an additional rail line from the intermodal terminal, across the Georges River, and through the Glenfield Waste Disposal facility, which would then branch into two lines that would connect with the Southern Sydney Freight Line (SSFL) now in construction. Glenfield Farm overlooks this area, and potential impacts to the Glenfield Farm SHR item include impacts to its views and setting, and a possible increase in noise from activity along the proposed new rail lines and the SSFL.

Recommendations

On the basis of background research and a site inspection and adhering to statutory obligations, it is recommended that;

- There are no non-Indigenous heritage constraints for the land within the proposed rail corridor, or the land within the Glenfield waste depot. The majority of these areas are heavily disturbed and do not contain known items of non-Indigenous heritage significance.
- There are no non-Indigenous heritage constraints on the proposal with regard to the heritage listed items of Kitchener House, the Holsworthy Group, Casula Powerhouse, and railway viaducts on the Southern Railway Line.
- The DNSDC site is highly significant and embodies important national heritage values, as indicated by its inclusion on the Commonwealth Heritage List. It is necessary to conserve the site's heritage values where possible. The site will no longer be protected under the EPBC Act once Defence's lease of the SIMTA site ends. It is therefore recommended that



discussions are commenced with the appropriate heritage bodies regarding the listing of the site on the NHL or the SHR.

- The actions necessary before heritage impacts can occur at the SIMTA site will depend on the statutory context of the site at the time that approval is sought for each stage of the SIMTA proposal. A SoHI should be produced for each stage of the Project Application process, and each SoHI should address the legal status of the site and provide advice on required actions depending on whether the site is listed on the CHL, NHL, SHR, or unlisted at the time that approval is sought.
- It is recommended that an overall mitigation strategy should be developed for the DNSDC site, which may be based on Table 3 of this report.
- Further archaeological assessment and possible investigation or monitoring will be required in areas designated as having archaeological potential, where they would be impacted by the intermodal terminal development. The SoHIs for each stage of the Project Application process should address the necessary actions regarding areas of archaeological potential within the development area for each stage of the SIMTA proposal.
- A Statement of Heritage Impacts should be prepared for Glenfield farm during the staged Project Application for the rail corridor adjacent to it, as the proposed development may impact on views and setting of this state significant site.
- If any archaeological deposit or item of heritage significance is located within the study area and is at risk of being impacted, the NSW Heritage Council should be notified and a heritage consultant/archaeologist should be engaged to assess the item to determine its heritage significance.
- As this project will be assessed under transitional arrangements for Part 3A of the *Environmental Planning and Assessment Act 1979*, permits and consents will not be required from the NSW Heritage Branch as a delegate of the NSW Heritage Council to impact on heritage items within sections of the study area not owned or leased by the Commonwealth.
- As part of the Project Applications stage a Statement of Commitments relating to non-Indigenous Heritage should be produced for the study area.

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1.0 Introduction and Background

The Sydney Intermodal Terminal Alliance (SIMTA) is a consortium of Qube Logistics and QR National. The SIMTA Moorebank Intermodal Terminal Facility (SIMTA proposal) is proposed to be located on the land parcel currently occupied by the Defence National Storage and Distribution Centre (DNSDC) on Moorebank Avenue, Moorebank, south west of Sydney. SIMTA proposes to develop the DNSDC occupied site into an intermodal terminal facility and warehouse/distribution facility, which will offer container storage and warehousing solutions with direct rail access to Port Botany. Construction of the rail connection from the SIMTA site to the Southern Sydney Freight Line (SSFL) will be undertaken as part of the first stage of works for the SIMTA proposal.

The SIMTA site is located in the Liverpool Local Government Area. It is 27 kilometres west of the Sydney CBD, 17 kilometres south of the Parramatta CBD, 5 kilometres east of the M5/M7 Interchange, 2 kilometres from the main north-south rail line and future Southern Sydney Freight Line, and 0.6 kilometres from the M5 motorway.

The SIMTA site, approximately 83 hectares in area, is currently operating as a Defence storage and distribution centre. The SIMTA site is legally identified as Lot 1 in DP1048263 and zoned as General Industrial under Liverpool City Council LEP 2008. The parcels of land to the south and south west that would be utilised for the proposed rail link are referred to as the rail corridor. The proposed rail corridor covers approximately 75 hectares and adjoins the Main Southern Railway to the north and south. The rail line is approximately 3.5 kilometres in length, 20 metres in width (variable width) and includes two connections to the SSFL, one south and one north.

The proposed rail corridor is owned by third parties, including the Commonwealth of Australia, RailCorp, private owners and Crown Land held by the Department of Primary Industries, and would link the SIMTA site with the Southern Sydney Freight Line. Existing uses include vacant land, existing rail corridors (East Hills Railway and Main Southern Railway), extractive industries, and a waste disposal facility. The rail corridor is intersected by Moorebank Ave, Georges River and Anzac Creek. Native vegetation cover includes woodland, forest and wetland communities in varying condition. The proposed rail corridor is zoned partly 'SP2 Infrastructure (Defence and Railway)' and partly 'RE1 - Public Recreation'. The surrounding Commonwealth lands are zoned 'SP2 Infrastructure (Defence)'.



A Concept Plan approval is being sought under the transitional provisions relating to Part 3A assessments under the Environmental Planning and Assessment Act 1979 (EP&A Act). On approval of the Concept Plan for the SIMTA proposal, staged development applications will be submitted to the NSW Department of Planning and Infrastructure as State significant development (SSD) under Part 4 of the EP&A Act. The first stage of development will include the rail link connection to the South Sydney Freight Line and on-site rail sidings within the DNSDC site.

This report provides an overall non-Indigenous heritage assessment, for the proposal as a whole, to support the application for Concept Plan approval as a transitional Part 3A project under the *Environmental Planning and Assessment Act 1979*. This assessment will fulfil the Director-General's Requirements for non-Indigenous heritage for the Concept Plan, as follows:

Table 1: Fulfilment of DGRs

Director-General's Requirements	Where addressed
Identify areas and items of non-indigenous heritage significance that could be impacted directly or indirectly, including potential archaeological deposits and the Australian Army Engineers Group and Kitchener House (formerly Arpafeelie) and an appropriate assessment of potential impacts (including site surveys)	Section 5.0 Section 6.0 Section 7.0
Detail how any impacts on items of indigenous and non-indigenous heritage would be addressed and managed as part of the subsequent project stages	Section 7.0 Section 9.0

1.1 The Proposed Development

The Concept Plan application comprises four key components:

- Rail Corridor.
- Intermodal Terminal.
- Warehouse and Distribution Facilities.
- Ancillary Terminal Facilities.

Each of these components is described briefly in the sections below.

Rail Corridor and Rail Link

The proposed rail link is proposed to connect to the Southern SSFL, approximately 500 metres south of Casula railway station. It would then extend south, then east, crossing Georges River from the south-east corner of the Glenfield Waste Disposal Centre. The rail link would then continue east within the East Hills rail corridor, before heading north into the SIMTA Site.

The proposed rail link would be constructed over the following parcels of land:

- SSFL rail corridor on the western side of the Georges River.
- Glenfield Waste Disposal Centre on the western side of the Georges River.
- East Hills rail corridor.
- Irregular shaped portion of land owned by RailCorp and located to the east of the intersection between Moorebank Avenue and the East Hills Railway Line.
- Land to the south of the DNSDC site owned by the Commonwealth.

The proposed rail link would include the following infrastructure:

- Culvert crossing of Anzac Creek.
- A crossing under Moorebank Avenue in proximity to the existing grade-separated crossing which supports the existing East Hills Railway Corridor.
- Bridging the Georges River.

The indicative rail link alignment is shown in Figure 2.

Intermodal Terminal

The intermodal terminal is proposed to be located on the western part of the site, adjacent to Moorebank Avenue and away from the nearest residential properties. Key elements include:

- Five rail tracks of approximately 650 to 1,200 metres in length, including four permanent and one temporary siding.
- Container hardstand of approximately 90,000m² located on both sides of the rail tracks to be used for container sorting and storage.
- Terminal administration offices and ancillary operational facilities of approximately 2,100m².



- The intermodal terminal is proposed to operate 24 hours a day, 7 days a week to enable continuous receipt and dispatch of freight, accommodating a wide range of servicing demands. It will be serviced by world class and leading practice intermodal facilities including:
 - Automatic gantry systems
 - Modern container handling equipment
 - Modern control tower and support facilities
 - State-of-the-art rolling stock

The final selection of mobile and static equipment will be made at the detailed application stage for the rail terminal, taking into account compliance with the criteria established by way of the Concept Plan approval, including noise levels, visual impacts and air quality.

Warehouse and Distribution Facilities

Approximately 300,000m² of warehouses with ancillary offices are proposed to be constructed to the east of the intermodal terminal. The proposed warehouses are to be sited and designed to provide a physical barrier between the intermodal terminal and the nearest residential properties to assist with mitigating the potential acoustic and visual impacts of the rail activities. These warehouses include:

- Intermodal Terminal Warehouse and Distribution Facilities (Terminal Warehouses) – approximately 100,000m² of warehouse floorspace will be located immediately adjacent to the intermodal terminal. These buildings will be designed for cross-dock operations and are anticipated to be occupied by large logistics operators dispatching goods in short turn-around times and with limited freight break-down.
- Large Format Warehouse and Distribution Facilities - approximately 200,000m² of warehouse floorspace will be located on the eastern part of the SIMTA site, east of the Terminal Warehouse facilities. These buildings will have perimeter loading docks and are anticipated to be occupied by logistics operators who require larger areas for operations, hold stock for longer periods and/or undertake larger amounts of freight-breakdown before dispatching.

Each of the warehouses will be serviced by the central internal road system. The road system design and location of the car park to the east of the large format warehouse buildings are proposed to maximise the separation of staff and freight vehicle movements and minimise potential vehicle conflicts.

Ancillary Terminal Facilities

A range of ancillary support facilities are proposed within the SIMTA Intermodal Terminal Facility to meet the needs of employees and visitors to the site. The final composition of these facilities will be based on demand and will be privately operated by individual tenants, however, it is anticipated that a total floorspace of approximately 8,000m² will be provided and the uses are likely to include:

- Site management and security offices.
- Retail and business service centre, potentially including a convenience store, banking facilities and post office.
- Meeting rooms/conference facilities available for hire by individual tenants.
- Sleeping facilities for drivers.
- A café/restaurant.

A centralised staff car parking area provided adjacent to the ancillary facilities will enable separation of heavy vehicle movements from private vehicle movements, particularly around the intermodal terminal warehouses.

Staging

The SIMTA Moorebank Intermodal Terminal Facility is proposed to be constructed in three stages, with the different stages as follows:

- Stage 1 – Construction of the intermodal terminal and rail link.
- Stage 2 – Construction of warehouses and distribution facilities.
- Stage 3 – Extension of the intermodal terminal and completion of warehouses and distribution facilities.

Figure 1: Map of proposed land uses

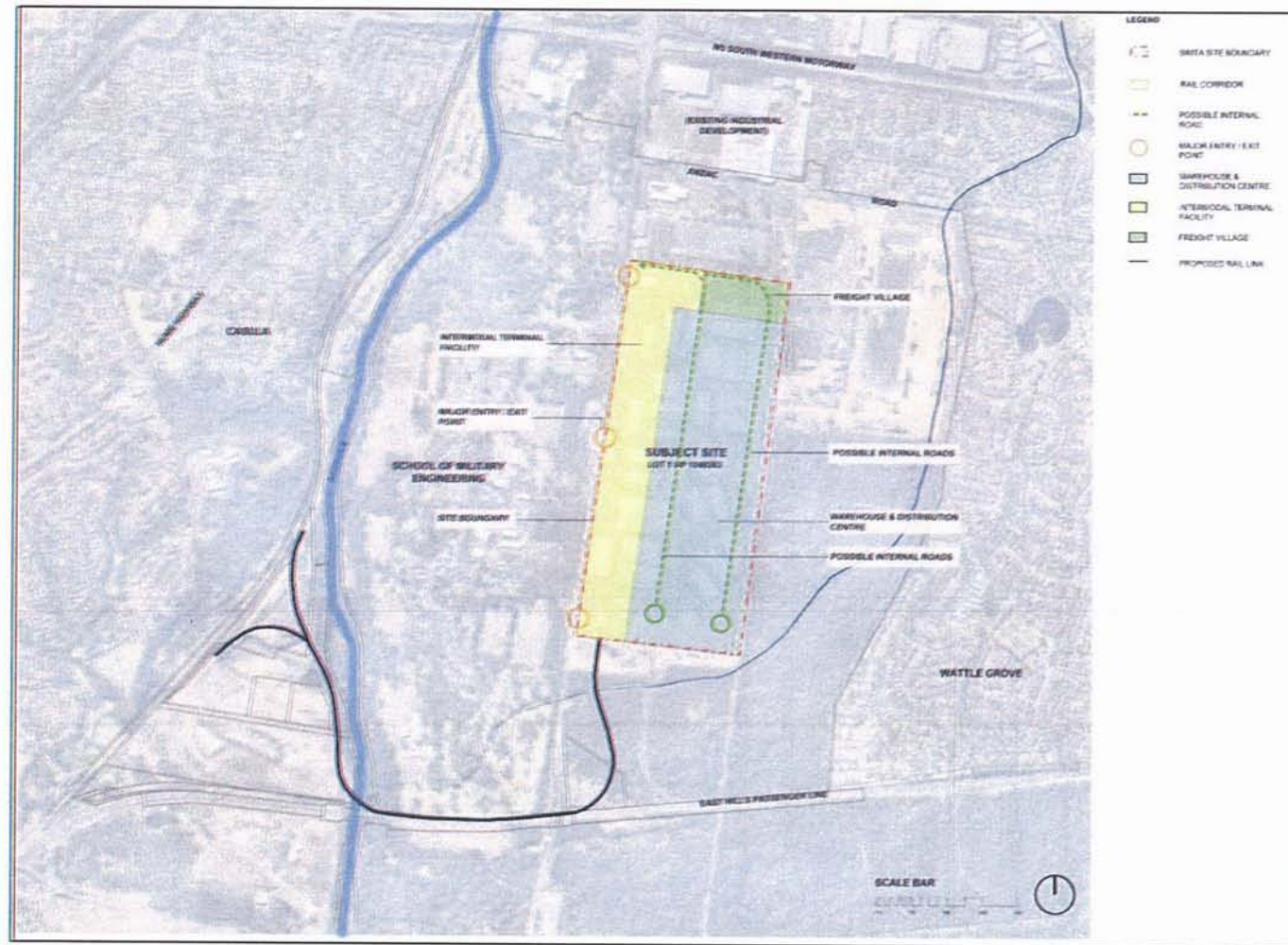
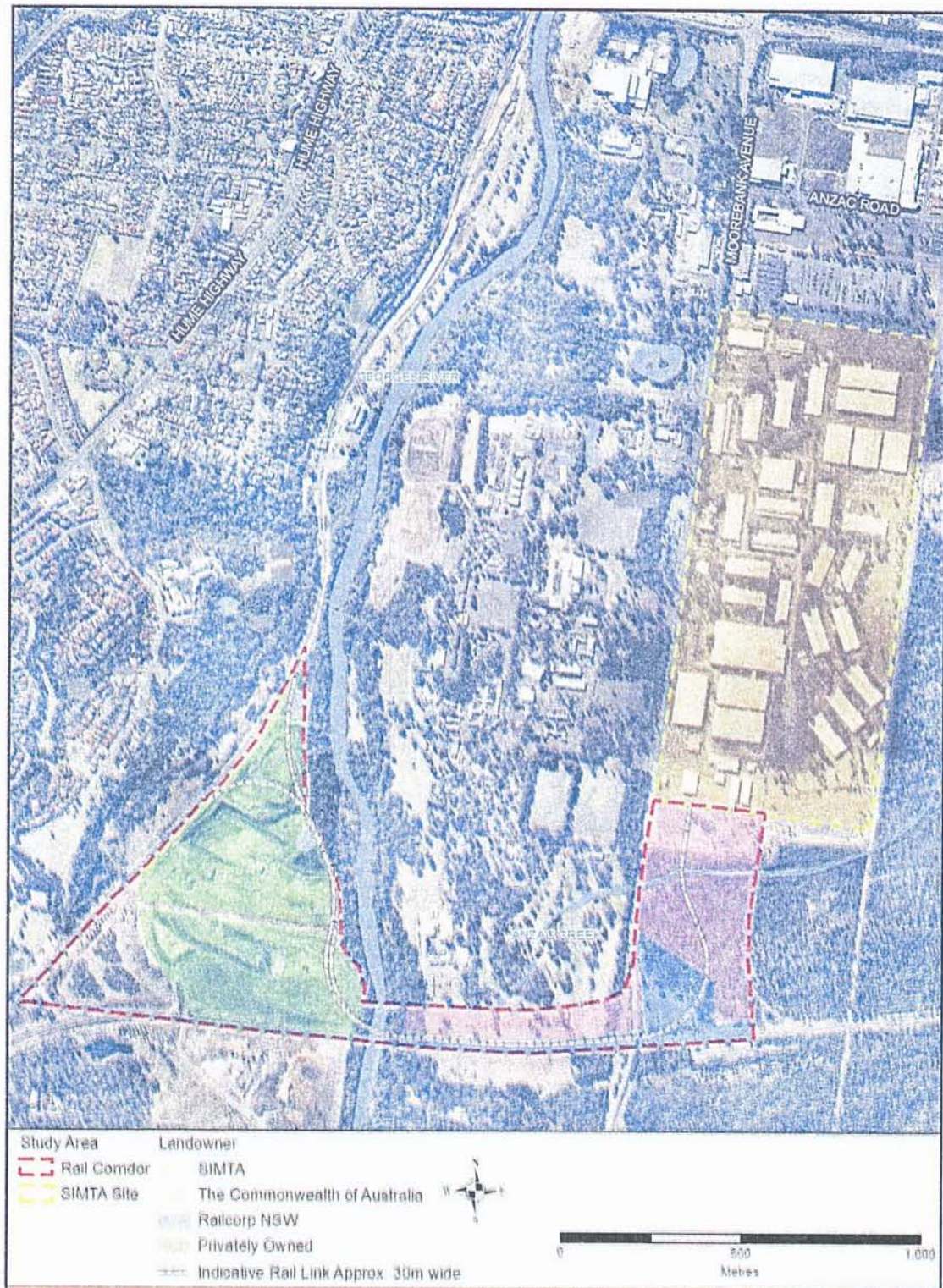


Figure 2: The dashed line indicates the boundary of the study area



1.2 Report Authorship

Archaeologist Adele Anderson and Principal Archaeologist Dr Sandra Wallace wrote this report. The assistance of Rebecca Sommer of Hyder Consulting is acknowledged in supplying relevant plans and other information.

2.0 Assessment Methodology

The methodology for this assessment included an initial search of heritage registers and documentary research, followed by a site survey to ground truth the desktop assessment and to identify and inspect any visible heritage items.

Heritage Register Search

Previously identified heritage items in the study area were located through a search of heritage registers, including:

- National Heritage List.
- Commonwealth Heritage List.
- Register of the National Estate.
- State Heritage Register.
- State Heritage Inventory.
- Section 170 Registers.
- Liverpool Local Environmental Plan 2008.
- Liverpool Development Control Plan 2008.

Documentary Research

Documentary research was conducted to investigate the general history of the locality, as well as the history of the study area itself, and of identified heritage items within it. The following libraries and archives were consulted:

- Liverpool Library, Local Studies Collection.
- National Library of Australia.
 - Maps (accessed through <http://www.nla.gov.au/digicoll/maps.html>).
 - Newspaper archives (accessed through <http://trove.nla.gov.au/ndp/del/search?adv=y>).
- Department of Lands.
 - Parish Map Preservation Project.
 - Aerial Photographs.
 - Spatial Information Exchange.
 - Old Title Records.



- National Archives of Australia.
- Australian War Memorial digital collection (<http://www.awm.gov.au/search/collections/>).

Site Survey

The site survey was undertaken by Dr Sandra Wallace and Adele Anderson (Artefact) on 13 July 2011. The survey was necessary to ground truth the desktop assessment and to investigate any heritage items in the study area.

The size of the study area, and its use by the military, meant that some parts of the SIMTA site could only be surveyed from a vehicle. However, areas where possible features were suggested by the documentary evidence were examined on foot. In most areas of the SIMTA site, surface visibility was poor because of the bitumen and concrete pavements covering the ground surface. A representative sample of the WWII structures at the SIMTA site were examined, and photographs taken of significant structural elements. The landscape was examined for any sign of former roads or railway sidings, and any visible features were noted and photographed.

The southern part of the School of Military Engineering was examined for any signs of former structures or landscape features possibly associated with the military installations in the area. However, this part of the study area had been heavily modified during the construction of the Royal Australian Engineers golf course and no evidence for any heritage items was visible.

The Glenfield Waste Disposal facility was not surveyed. It has undergone significant landscape modification and disturbance and it is therefore unlikely that any heritage items or archaeological deposits are present in this area.

3.0 Legislative Framework

There are several pieces of State legislation and regulation that are relevant to the current study. A summary of these Acts and the implications for the SIMTA proposal follow. It should be noted that as the project will be assessed under Part 3A transitional arrangements some statutory obligations will not apply.

The Heritage Act 1977

The NSW *Heritage Act 1977* is the primary piece of heritage legislation affording protection to items of state heritage significance and archaeological material and deposits in New South Wales. Under the Act, 'items of environmental heritage' include places, buildings, works, relics, moveable objects and precincts identified as significant based on historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic values. Items of identified heritage afforded a level of State Significance are listed on the NSW State Heritage Register and are given automatic protection under the Act against any activities that may damage an item or affect its heritage significance.

If works are proposed within a State Heritage Register listed site consent is required under Section 60 of the Heritage Act. An exemption may be granted by the Heritage Branch.

The Heritage Act protects 'relics' as defined by the Act and such 'relics' include archaeological material. Section 139[1] of the Act states that:

A person must not disturb or excavate any land knowingly or having reasonable cause to suspect that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, damaged or destroyed unless the disturbance or excavation is carried out in accordance with an excavation permit.

Permits to excavate 'relics' which are not within an area listed on the State Heritage Register are issued by the Heritage Council under Section 140 of the Act. Exceptions may be made under certain conditions and would be approved in writing by the Heritage Council.

As this project is seeking approval under Part 3A transitional arrangements under the *Environmental Planning and Assessment Act 1979*, permits and consents from the Heritage Branch will not be required to impact heritage items.

Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act)

The *Environment Protection and Biodiversity Conservation Act 1999* (the EPBC Act) provides a legal framework to protect and manage nationally and internationally important flora, fauna, ecological communities and heritage places. These are defined in the EPBC Act 1999 as matters of national environmental significance. Under the EPBC Act 1999, nationally significant heritage items are protected through listing on the Commonwealth Heritage List or the National Heritage List.

According to the EPBC Act 1999, a person must not take an action that has, will have or is likely to have a significant impact on any of the matters of environmental significance without approval from the Australian Government Minister for Sustainability, Environment, Water, Population and Communities (the Minister). An action is defined as a project, a development, an undertaking, an activity or a series of activities, or an alteration of any of these things. If a proposed action is likely to have a significant impact on a nationally significant heritage item, a referral must be made to the Minister to seek approval.

NSW S170 Heritage and Conservation Registers

The NSW *Heritage Act 1977* also requires all government agencies to identify and manage heritage assets in their ownership and control. Under Section 170 of the Act, government instrumentalities must establish and keep a register which includes all items of environmental heritage listed on the State Heritage Register, an environmental planning instrument or which may be subject to an interim heritage order that are owned, occupied or managed by that government body. Under Section 170A of the *Heritage Act 1977*, all government agencies must also ensure that all items entered on its register are maintained with due diligence in accordance with State Owned Heritage Management Principles approved by the NSW Minister for Infrastructure & Planning on advice of the NSW Heritage Council. These principles serve to protect and conserve the heritage significance of identified sites, items and objects and are based on relevant NSW heritage legislation and statutory guidelines.

Environmental Planning and Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* establishes the framework for cultural heritage values to be formally assessed in the land use planning and development consent process. The Act requires that environmental impacts are considered prior to land development; this includes impacts on cultural heritage items and places as well as archaeological sites and deposits. The Act also requires that Local Governments prepare planning instruments (such as Local Environmental Plans, Development Control

Plans) in accordance with the Act to provide guidance on the level of environmental assessment required. The current study area falls within the boundaries of the Liverpool LGA and is within the area covered by the Liverpool Local Environmental Plan [LEP] (2008).

Liverpool Local Environmental Plan 2008

The aim of the LEP in relation to heritage, as stated in section 1.2 (g) is to conserve, protect and enhance the environmental and cultural heritage of Liverpool. The LEP lists items of heritage significance within the LGA and specifies conditions of development consent within heritage listed area. The relevant clauses of the LEP are as follows.

Requirement for consent

Development consent is required for any of the following:

- (a) demolishing or moving a heritage item or a building, work, relic or tree within a heritage conservation area,*
- (b) altering a heritage item or a building, work, relic, tree or place within a heritage conservation area, including (in the case of a building) making changes to the detail, fabric, finish or appearance of its exterior,*
- (c) altering a heritage item that is a building by making structural changes to its interior,*
- (d) disturbing or excavating an archaeological site while knowing, or having reasonable cause to suspect, that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, moved, damaged or destroyed,*
- (e) disturbing or excavating a heritage conservation area that is a place of Aboriginal heritage significance,*
- (f) erecting a building on land on which a heritage item is located or that is within a heritage conservation area,*
- (g) subdividing land on which a heritage item is located or that is within a heritage conservation area.*

(3) When consent not required

However, consent under this clause is not required if:

- (a) the applicant has notified the consent authority of the proposed development and the consent authority has advised the applicant in writing before any work is carried out that it is satisfied that the proposed development:*
 - (i) is of a minor nature, or is for the maintenance of the heritage item, archaeological site, or a building, work, relic, tree or place within a heritage conservation area, and*
 - (ii) would not adversely affect the significance of the heritage item, archaeological site or heritage conservation area,*
or
- (b) the development is in a cemetery or burial ground and the proposed development:*



- (i) is the creation of a new grave or monument, or excavation or disturbance of land for the purpose of conserving or repairing monuments or grave markers, and*
- (ii) would not cause disturbance to human remains, relics, Aboriginal objects in the form of grave goods, or to a place of Aboriginal heritage significance, or*
- (c) the development is limited to the removal of a tree or other vegetation that the Council is satisfied is a risk to human life or property, or*
- (d) the development is exempt development.*

(4) Effect on heritage significance

The consent authority must, before granting consent under this clause, consider the effect of the proposed development on the heritage significance of the heritage item or heritage conservation area concerned. This subclause applies regardless of whether a heritage impact statement is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).

(5) Heritage impact assessment

The consent authority may, before granting consent to any development on land:

- (a) on which a heritage item is situated, or*
- (b) within a heritage conservation area, or*
- (c) within the vicinity of land referred to in paragraph (a) or (b),*

require a heritage impact statement to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.

As the SIMTA project is seeking approval under transitional Part 3A arrangements, the Minister is not obliged to consider LEP requirements.

Liverpool Development Control Plan 2008

The Liverpool Development Control Plan (DCP) aims to conserve the heritage significance of heritage items and heritage conservation areas of Liverpool including associated fabric, setting, curtilage and views, and to conserve archaeological sites (DCP page 69). The DCP states that development applications relating to heritage items or places in the vicinity of a heritage item, require a Statement of Heritage Impact (DCP page 70). It also addresses the importance of setting, stating that development in the vicinity of a heritage item should retain significant views to and from the item, retain original landscaping, and provide an adequate area around the place to allow interpretation of the item (DCP page 73).

Part 2.4 of the DCP specifically addresses development on the Moorebank Defence Lands and states that an appropriate curtilage should be maintained around Kitchener House. In order to retain an appropriate visual setting, the scale and character of new development along Moorebank Avenue should respect that of Kitchener House, should not intrude within its curtilage, and should be screened by planting (DCP page 24).

As the SIMTA project is seeking approval under transitional Part 3A arrangements the Minister is not obliged to consider DCP requirements.

Implications of Legislation

As the SIMTA project is to be assessed under Part 3A transitional arrangements permits and consents under the *Heritage Act 1977* are not required to impact heritage items within sections of the study area not owned or leased by the Commonwealth. Under Part 3A transitional arrangements, consideration of the heritage obligations of the Liverpool LEP and the Liverpool DCP are at the discretion of the Minister.

Legal status of heritage items on the SIMTA site

The DNSDC is listed on the Commonwealth Heritage List (CHL) and is currently protected under the EPBC Act. However, items can only be included on the CHL while they are located within a "Commonwealth Area" (s341C (2) EPBC Act). The SIMTA site will only be located within a "Commonwealth Area" for as long as Defence leases the site, and once that lease expires or is relinquished, the SIMTA site would no longer be within a "Commonwealth Area" and would need to be removed from the CHL (s341L EPBC Act). It is possible that the site may then be considered for listing on another heritage register, such as the NHL or SHR. If either of these listings were to occur prior to the granting of development approval for the SIMTA site, SIMTA would be required to fulfil additional obligations under the relevant heritage legislation.

Management responsibilities for heritage items on the SIMTA site

The EPBC Act imposes obligations on the Commonwealth to prepare Heritage Management Strategies (HMSs) and Heritage Management Plans (HMPs) for places on the CHL that it "owns or controls" (s341S and s341ZA). The DNSDC is one such place, as Defence is considered to "control" a place if it has rights under a lease or license to occupy or use the place and to take actions in relation to the place that could potentially have an impact on its heritage values. The Commonwealth is required to act in accordance



with the HMSs and HMPs to minimise adverse impacts to the heritage values of listed places (s341V and s341ZC).

Defence has prepared a HMS which establishes Defence's overall approach to heritage management, however, it is not known whether a HMP has yet been prepared for the DNSDC. Even if a HMP does exist for the DNSDC, the obligation to comply with the plan will only apply to Defence while it leases the SIMTA site. SIMTA itself would only be bound by the Defence management documents if:

- SIMTA had previously agreed (by contract, deed, or other form of legally binding agreement) to be so bound; or,
- SIMTA agrees to enter into a binding agreement with Defence in the future, which includes obligations to protect heritage values.

Therefore, the ongoing heritage protection measures that will exist once Defence vacates the SIMTA site will entirely depend on:

- The terms of any contractual obligations between SIMTA and Defence that operate at that point in time; and,
- Whether the SIMTA site is subsequently listed on the NHL or SHR and thereby becomes subject to the regulatory requirements of the relevant legislation.

Conclusion

Before works begin on each stage of the SIMTA proposal, Defence will have vacated the relevant areas of the site. However, because approval is being sought at the present time, while the entire area is still leased by Defence, the approvals process will need to meet the requirements of the EPBC Act 1999.

A Commonwealth EIS must be submitted to the Australian Government Minister for Sustainability, Environment, Water, Population and Communities (the Minister) for approval, for each stage of the SIMTA proposal. A NSW State EIS must also be submitted to the NSW Minister for Planning and Infrastructure for approval for each stage.

4.0 Historical Context

4.1 Early settlement

The first land parcels in the Liverpool area were granted in 1798. In 1810 Governor Macquarie founded Liverpool and named it after the Earl of Liverpool. The road connecting Liverpool to Sydney was completed in 1813 and settlement grew rapidly. The rich soils on the floodplain of the Georges River provided for a growing agricultural industry. In the 1860s many small farmers moved away from the river after a particularly large inundation and the area became open to larger scale agriculture such as dairy farming. Up until the mid-twentieth century agriculture co-existed with suburban areas in the Liverpool region.

4.2 The military at Liverpool

The association of military activities with the Liverpool district began in the early 1800s, when soldiers were stationed in the area to provide protection to early settlers and to oversee convict work gangs, and a military barracks was constructed at the corner of George and Moore Streets (Brooks and Associates 2002:8).

During the early 1900s, the area north of the SIMTA site hosted several military training camps. These were held annually as part of the 'Easter Encampments', a training programme which also involved camps at Paddington and Goulburn (*The Sydney Morning Herald* (SMH) 27/3/1906:6). By 1907, a military camp had been established on the eastern side of the Georges River, with a rifle range further south. The land which is currently occupied by the DNSDC formed part of this camp (Brooks and Associates 2002:8).

In January 1910, manoeuvres were held at the Liverpool camp for the inspection of Lord Kitchener, who was visiting Australia to give advice regarding the development of the national defence forces (Brooks and Associates 2002:8). *The Daily Telegraph* described the area used for the manoeuvres:

"The camp was pitched upon the paddocks to the left of the railway station on the ground that has been similarly occupied in recent years and which is nearly all included in the military manoeuvre area which the Commonwealth Government is endeavouring to secure ... the training ground embraces a stretch of country extending from Liverpool, on the southern line, across Heathcote on the Illawarra system, and it provides not only very fair

opportunities for moving large bodies of troops in tactical exercises, but also has within its limits well equipped ranges for artillery and infantry shell and ball practice." (*The Daily Telegraph* 7/1/1910:7)

Kitchener recommended that large, central training grounds should be established in each State (SMH 19/2/1910:12). His visit resulted in the acquisition of large areas of land around Liverpool by the Government, for use as permanent military training camps. The land was resumed in stages over the following years and included the acquisition of 883 acres near Holsworthy in 1912 for the establishment of a Remount Depot and a Veterinary Hospital for horses, followed by 16,868 acres in 1913, which included the study area (Brooks and Associates 2002:4).

World War One

By 1913, the Liverpool camp accommodated 2000 troops in tents (SMH 3/1/1913:10), and during WWI it became the main training centre in New South Wales. In a plan dated to 1915, Liverpool Camp is located between the Georges River and Moorebank Avenue, and extends around 1.5 kilometres south from Illawarra Road, which was located in roughly the same position as the present Newbridge Road. South-east of the camp are large areas marked 'Stores', which encompass the current DNSDC site. East of the storage area is a rifle range.

Initially, new recruits were encamped in long lines of tents on the eastern bank of the river, though these had been replaced with huts by the end of 1916. A detailed plan of the camp from July 1917 shows that the camp was well established and included a large number of huts, kitchens, and mess buildings, as well as a saw mill, four church buildings, a post office, bank, power house, Y.M.C.A building, hospital buildings, nurses quarters, and buildings for the salvation army and the Red Cross.

Units that trained at the camp during the WWI included the Engineer and Field Mining companies, the field hospital, infantry and reinforcement units, and the artillery and light horse units.

Figure 3: Plan of the Liverpool Manoeuvre Area c.1915 (Source: Brooks & Associates 2002:7)



Figure 4: A recruit marking tent line boundaries at the Liverpool camp c. 1914 (Source: Australian War Memorial [AWM], ID No: H03409)

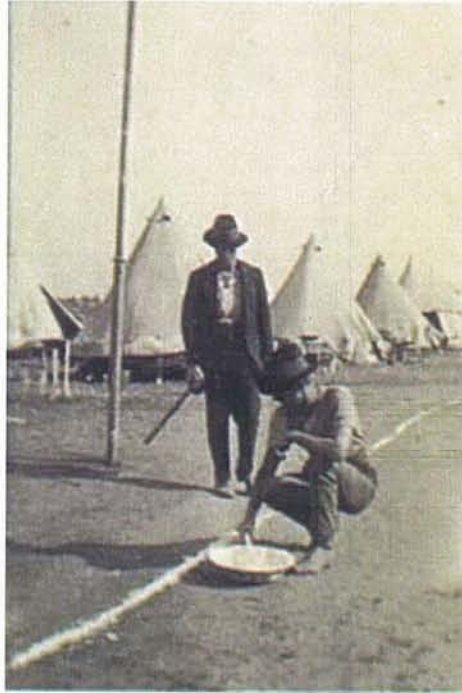
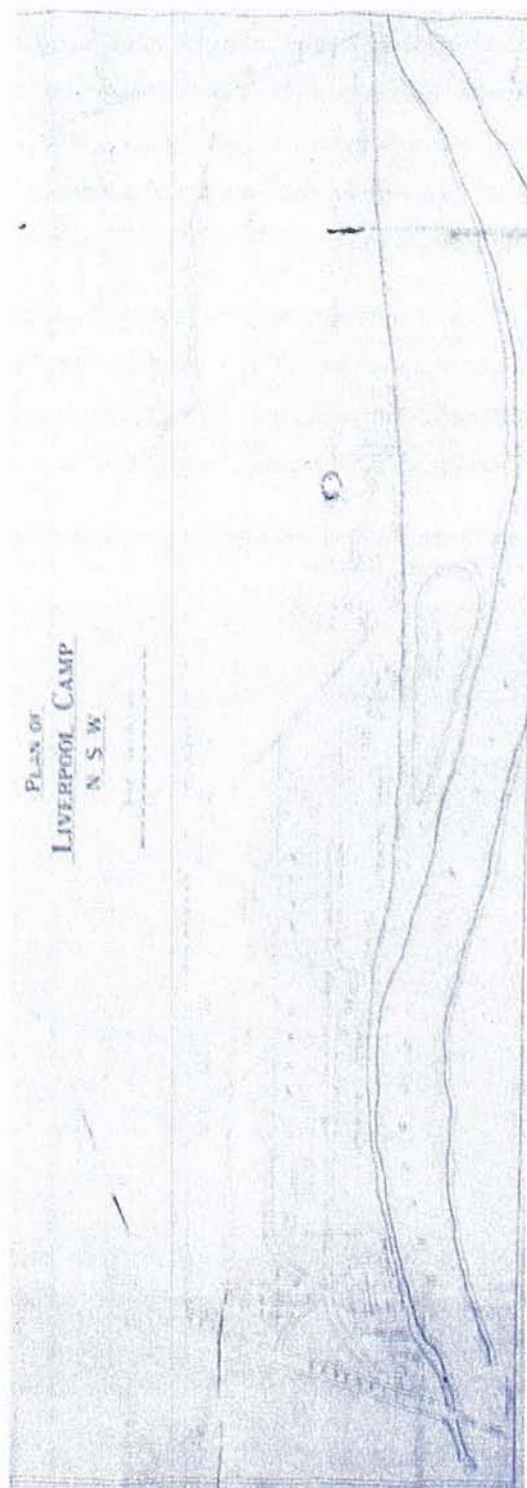


Figure 5: Accommodation huts, Oct 1916 (Source: AWM, ID No: C01205)



Figure 6: Plan of Liverpool Camp, 1917 (Source: Liverpool City Council
<http://ebranch.liverpool.nsw.gov.au/electronicbooks/Subdivisionplans.pdf>)

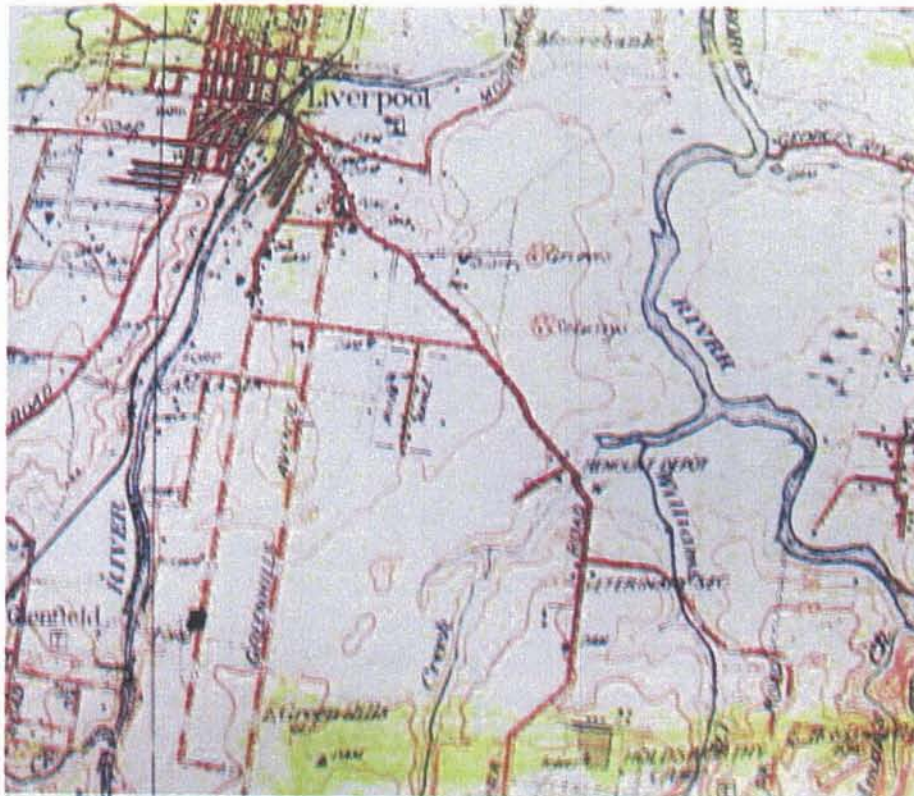




In 1913, a Remount Depot had been established at Holsworthy, approximately 4 kilometres south-east of the Liverpool camp. The Remount Branch of the Australian Military Forces had been established in 1911 and was responsible for purchasing, breaking in, and caring for military horses. Initially, the Remount Depot at Holsworthy mainly supplied horses for artillery and transport, but during WWI it provided mounts for the enlisted Light Horsemen who came from other parts of NSW and Queensland to enrol, train, and embark from Sydney. By 1914, a Veterinary Section was also established at Holsworthy, to care for the horses (Ludlow & Snowden 1991:64-5).

Also located at Holsworthy was a large internment camp for 'enemy aliens' and prisoners-of-war, which became known as the German Concentration Camp. The area occupied by the camp was never clearly defined, but measured approximately 1.5 kilometres by 1 kilometre, and was located south of the Remount Depot and Veterinary Section (Godden Mackay Logan 1995:2/1).

Figure 7: 1917 plan showing Liverpool camp, the Remount Depot, the Veterinary Section, and the Holsworthy internment camp (Source: Ludlow & Snowden 1993:56)



Internees from the German Concentration Camp assisted in the construction of new railway lines to link the different military establishments at Liverpool and Holsworthy (Ludlow & Snowden 1993:62). The Government wanted the new lines to service the Liverpool camp, the Artillery Range to its east, ordnance and ammunition stores two miles from the main camp, the Remount Depot, Veterinary Section, and German Concentration Camp (Ludlow & Snowden 1993:60). Construction of the line began in February 1917 and was completed in January 1918, with additional sidings added in the following years. First the Ordnance Store Siding opened in April 1919, followed by the Ammunition Stores Siding on Anzac Parade, opened in October 1920 (Ludlow & Snowden 1993:60-1).

Figure 8: Construction of a railway cutting near the German Concentration Camp by internees, 1917 (Source: Oakes 1997:2)



World War Two

The facilities at Liverpool and Holsworthy continued to be used for military training during the interwar years, although on a much reduced scale, before the beginning of WWII necessitated the nation-wide expansion of sites associated with defence training, manufacture, and storage. In the Liverpool area there was an enormous expansion of army installations, with about 40,000 troops in-training at Liverpool, Holsworthy, and Ingleburn (Department of Defence 'History of the 5th Brigade' http://www.army.gov.au/HQ5BDE/Unit_History.asp. Accessed: 16/7/11)

The School of Military Engineering was established to the south of Liverpool camp in 1939, immediately after the declaration of war. During the war 7,450 students were trained at the school (Liverpool Library Local Studies pamphlet *'The Army at Liverpool'*). By 1943, the area of Liverpool camp between the Georges

Figure 9: Plan of Liverpool military area 6/10/1943, red arrows indicate the Liverpool camp area, the AFVTTC base, and the School of Military Engineering (Source: NAA: SP459/1, 420/7/1153)

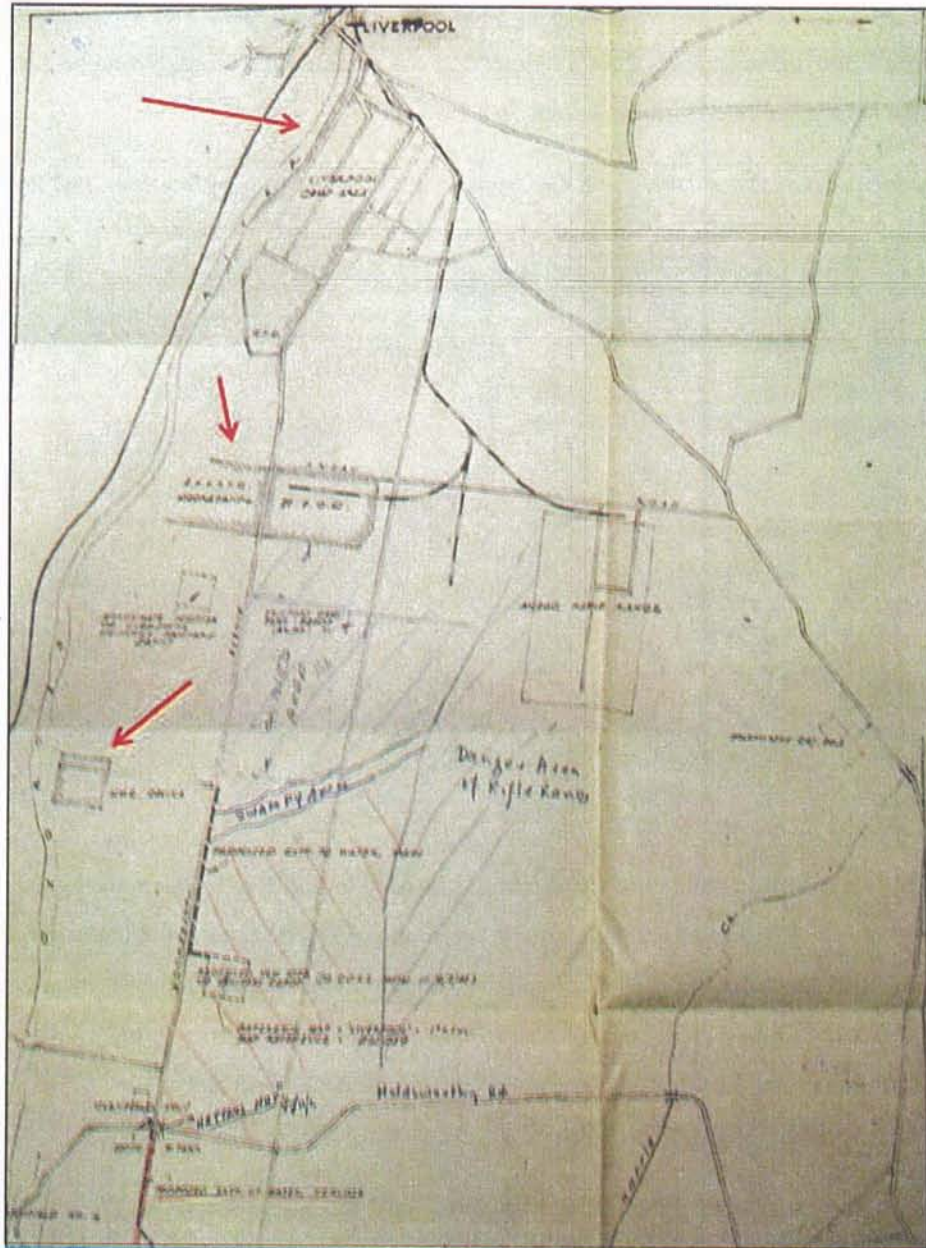
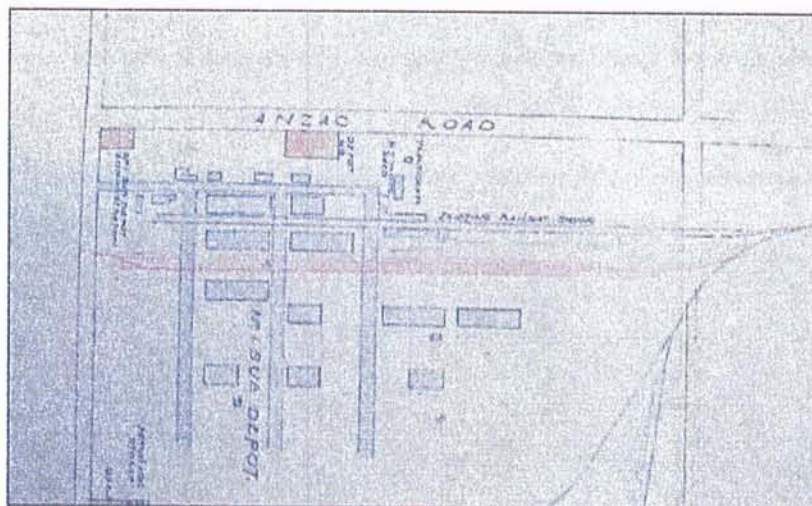


Figure 10: Detail of No. 1 Sub depot on corner of Anzac Rd and Moorebank Avenue 16/9/43 (Source: NAA: SP459/1, 420/7/1153)



In September 1943, it was proposed that Ordnance Stores should be established at Moorebank for the 5th Australian Base Ordnance Depot (5 Aust. BOD) and by December a plan for the proposed layout of the Ordnance Depot had been drawn up. In January 1944, urgent approval was sought for the construction of four of the proposed storehouses (Numbers 10, 11, 12 and 13) due to a shortage of storage facilities in the area (*Letter from Quarter-Master General 11/1/44*, NAA: SP459/1, 420/7/1153). Approval was granted in February, and these buildings formed the first construction phase of the depot, now known as the DNSDC (*Letter from Quarter-Master General 16/2/44*, NAA: SP459/1, 420/7/1153). Buildings 10 and 11 are still present at the DNSDC site. The completed depot was to include:

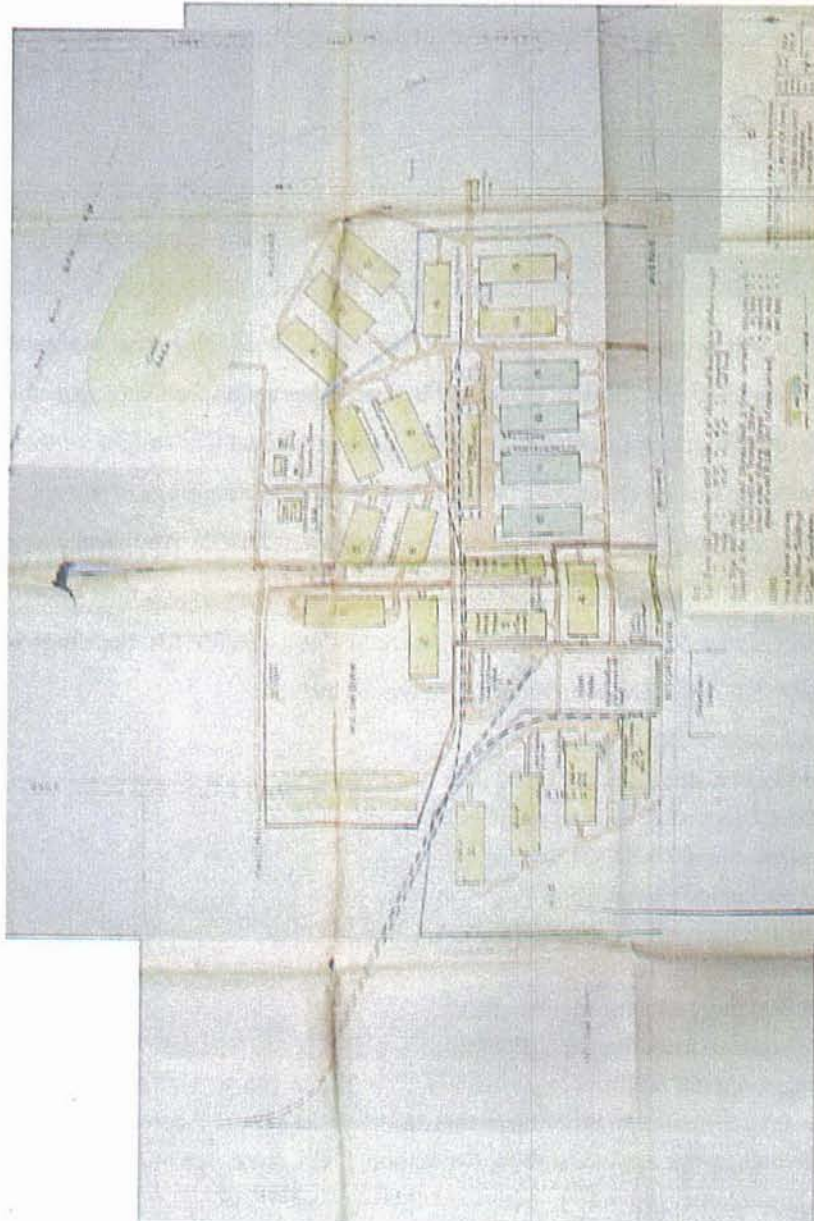
- 17 stores (400' x 150' in size).
- 2 crane served stores (400' x 150').
- 19 offices attached to each store (40' x 20').
- 1 transit store (500' x 83'4").
- Office acc. inside transit store.
- 1 cinematograph store (60' x 40').
- 2 inflammables stores (100' x 50').
- 20, 000 square feet of equipment shelters.
- 1 traffic control building (18' x 17'8").
- 1 strong room (50' x 50').
- 1 Depot Administration building in three blocks (135'4" x 111' combined size).
- 1 combined garage, service station, fire station, P.O.L store, Tpt office (97' x 25').
- 1 SW guard house (60' x 20').
- 1 case making building (3,750 square feet).
- 7 men's latrines.

...

- 3 AWAS latrines.
 - 3 AWAS latrines and rest rooms.
- (NAA: SP459/1, 420/7/1153)

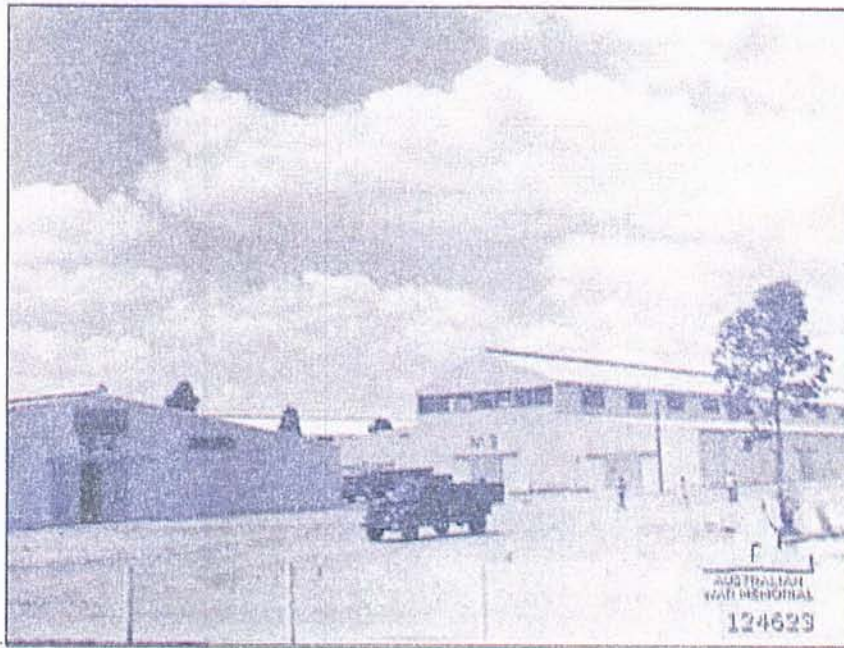
It was intended that the depot would have an ongoing role in peace-time as well as war-time (*Letter from Colonel Garnsey 5/4/44, NAA: SP459/1, 420/7/1153*).

Figure 11: Plan of proposed layout of Moorebank Ordnance Depot 25/4/44 (Source: NAA: SP459/1, 420/7/1153)



In April 1944, the AFVTTC transferred to the Ingleburn army camp and the vacated Liverpool camp buildings to the west of Moorebank Avenue were then used to accommodate the personnel of 5 Aust. BOD, as well as the 8th Australian Advanced Workshops of the AEME, who had been transferred from Bathurst. By 1945, the Australian Women's Army Service (AWAS) was also housed here (NAA: SP459/1, 420/7/1153).

Figure 12: 5th Aust. BOD exterior view of No. 9 Bulk (Crane Served) Technical Store Shed, 23/1/46 (Source: AWM, ID No. 124623)



Aerial photographs of the DNSDC site show that little change occurred between the late 1940s and early 1990s, when five of the original 20 store buildings (in the south-west corner) were demolished and replaced with larger modern buildings (Figure 15). The remaining 15 store buildings were also reclad at this time, with modern steel sheeting replacing the original asbestos walls and new concrete floors laid (Brooks and Associates 2002:8).

In the early 1990s, the site became the Defence National Storage and Distribution Centre, as part of a reorganisation of defence supply services and warehousing arrangements. The DNSDC is the central warehouse for Australia's armed services, and also includes maintenance and engineering facilities (Brooks and Associates 2002:9).

Figure 13: Aerial photograph showing the Ordnance Depot/DNSDC site in 1951 (Source: Brooks & Associates 2002:9)

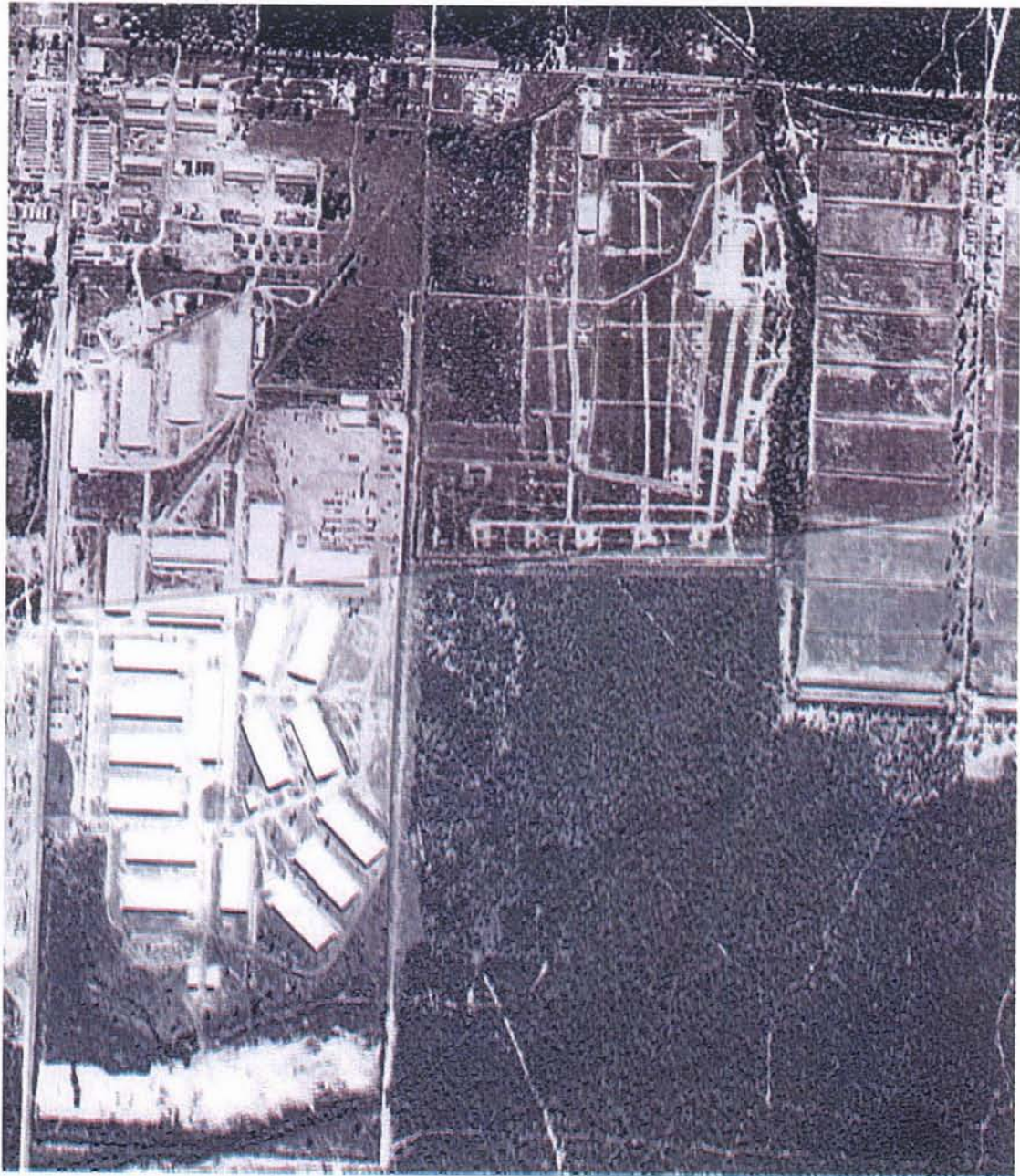
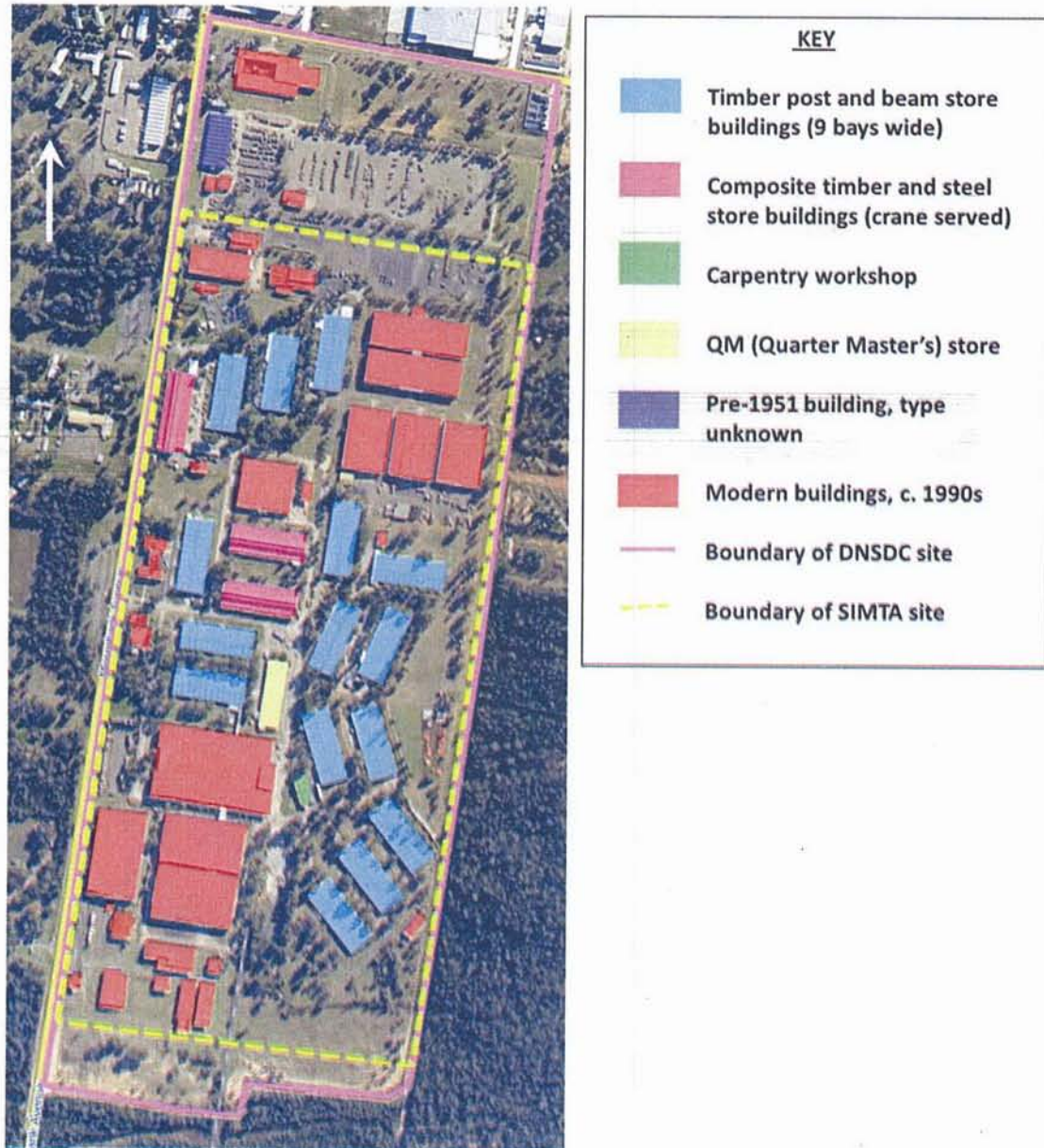


Figure 14: Aerial photograph of the DNSDC site 2011 (Source: Google Earth)



Figure 15: Plan showing the current location of building types within the DNSDC site.



5.0 Register Listings

Statutory registers provide legal protection for heritage items. In NSW the *Heritage Act 1977*, and the *Environmental Planning and Assessment Act 1979* give legal protection. The State Heritage Register, the S170 registers, and heritage schedules of Local Environment Plans are statutory listings. Places on the National Heritage List and the Commonwealth Heritage List are protected under the *Environment Protection and Biodiversity Conservation Act 1999*.

Commonwealth Heritage List

The Commonwealth Heritage List, established under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act), is a list of natural, Indigenous and historic heritage places which are either entirely within a Commonwealth area, or outside the Australian jurisdiction and owned or leased by the Commonwealth or a Commonwealth Authority. These include places connected to defence, communications, customs and other government activities that also reflect the development of the nation. To be entered on the Commonwealth List, a place must have 'significant' heritage value to the nation. Items on the list are under statutory protection.

The DNSDC site encompassing the SIMTA site is listed on the Commonwealth Heritage List. Although it is no longer owned by the Commonwealth, the site is under lease to the Australian Defence Force and will therefore remain protected under the EPBC Act 1999 until this lease expires.

Register of the National Estate

The Register of the National Estate is a list of natural, Indigenous and historic heritage places throughout Australia. It was originally established under the *Australian Heritage Commission Act 1975*. Under that Act, the Australian Heritage Commission entered more than 13,000 places in the register. Following amendments to the *Australian Heritage Council Act 2003*, the Register of the National Estate (RNE) was frozen on 19 February 2007, which means that no new places can be added, or removed. The Register will continue as a statutory register until February 2012.

On 1 January 2004, a new national heritage system was established under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act). This led to the introduction of the National Heritage List, which was designed to recognise and protect places of outstanding heritage value to the nation.

Kitchener House is included in the Register of the National Estate, while the DNSDC is included on the interim list of the Register. This means that it had been publicly proposed for entry in the Register and was on the Interim List at 1 January 2004 when the Australian Heritage Commission was abolished.

The nearby sites of Glenfield Farm and the Holsworthy Group are also listed on the Register.

National Heritage List

The National Heritage List has been established to list places of outstanding heritage significance to Australia. It includes natural, historic and Indigenous places that are of outstanding national heritage value to the Australian nation.

No sites in or near the study area are included on the National Heritage List.

Section 170 Registers

Section 170 requires government agencies to keep a Register of heritage items. A S.170 Register is a record of the heritage assets owned or managed by a NSW government agency. Relevant s170 registers were checked (Sydney Water, RTA, Railcorp).

No s170 register listings were found within the study area, but the nearby railway viaducts at Woodbrook Road & Congressional Drive, Casula, are listed on RailCorp's s170 Register.

The State Heritage Register

The State Heritage Register is a list of places and objects of particular importance to the people of NSW and is administered by the Heritage Branch of the Office of Environment and Heritage. The register lists a diverse range of over 1,500 items, in both private and public ownership. To be listed, an item must be deemed to be of heritage significance for the whole of NSW.

Glenfield Farm, adjacent to the Glenfield Waste depot is listed on the State Heritage Register.

Liverpool Local Environmental Plan (LEP) 2008

The Liverpool LEP includes a list and maps of items/sites of heritage significance within the LGA. Only one of these items, the Australian Army Engineers Group (or School of Military Engineering), falls within the study area. Six other listed items are located in the vicinity of the study area.

Moorebank, Proposed Intermodal Terminal - Heritage Assessment

Table 2: Heritage items within and near the study area - Liverpool LEP

Suburb	Item	Within the study area?	Lot/DP	Significance	LEP Item number
Moorebank	Australian Army Engineers Group.	Yes, southern section only	Lots 3001-3005, DP 1125930	Local	57
Casula	Casula Powerhouse (former power station)	No	Lots 1 and 2, DP 106957; Lot 1, DP 1115187	Local	10
Casula	Railway viaduct	No	N/A Located 300m south of Casula Powerhouse, Main Southern Railway Line	Local	11
Casula	Two railway viaducts	No	N/A Located Woodbrook Road, Main Southern Railway Line	Local	12
Casula	Glenfield Farm Group, including homestead, barn (former dairy and stables)	No	Lots 1 and 2, DP 1126484	State	14
Holsworthy	Holsworthy Group, including powder magazine and former officers' mess, corporals' club, internment camp, Holsworthy railway station lock-up/gaol, German concentration camp	No	Lot 1, DP 825745; Part Lot 820, DP 1011240; Lot 2, DP 1048198; Part Lot 32, DP 848597; Part Lot 10, DP 1091209	State	32
Moorebank	Kitchener House (formerly 'Arpafeelie')	No	Lot 1001, DP 1050177	Local	58

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Figure 16: Liverpool LEP Heritage Map (Sheet HER_013)

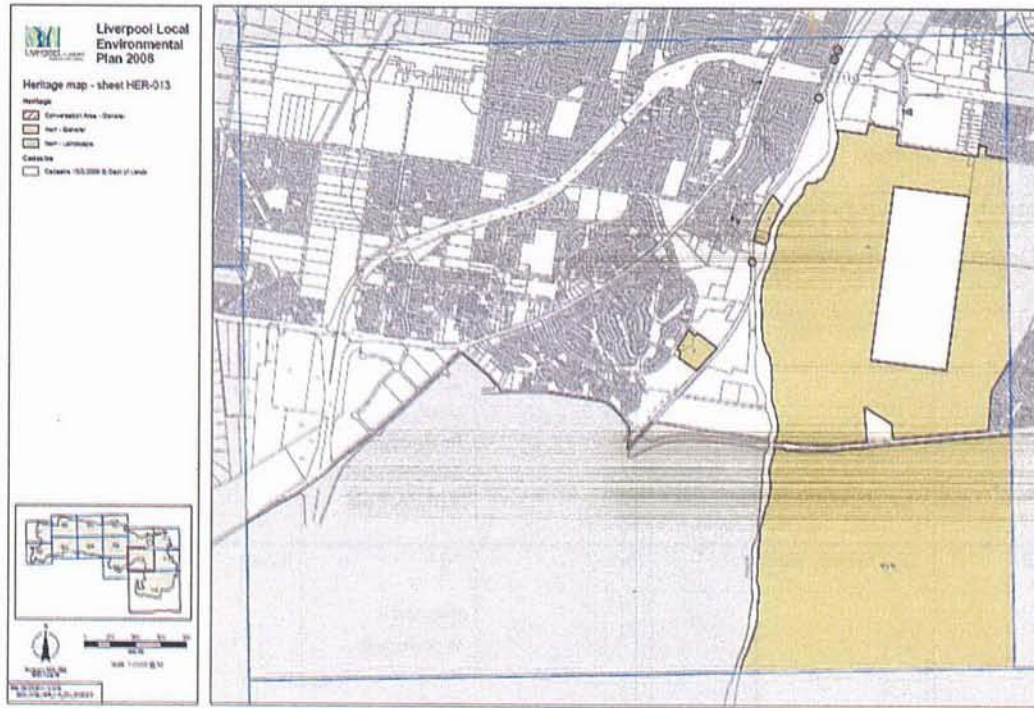
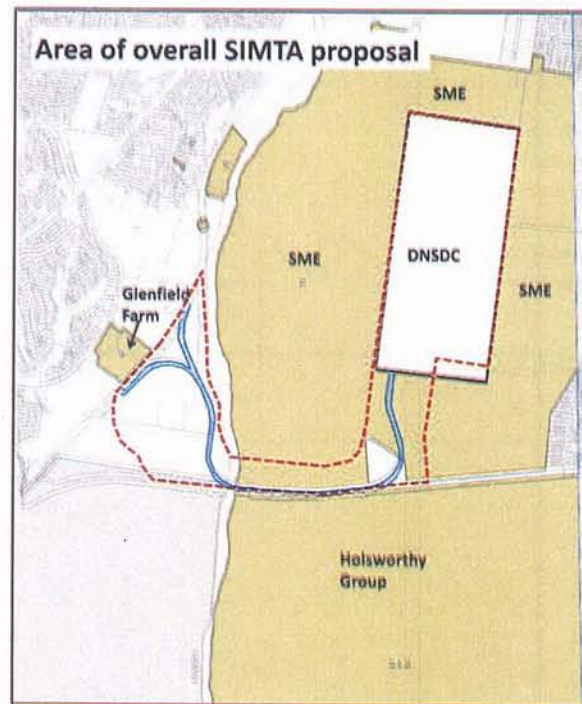


Figure 17: Detail from Liverpool LEP Heritage map, with boundaries of study area in red and proposed rail link in blue (Sheet HER_013)



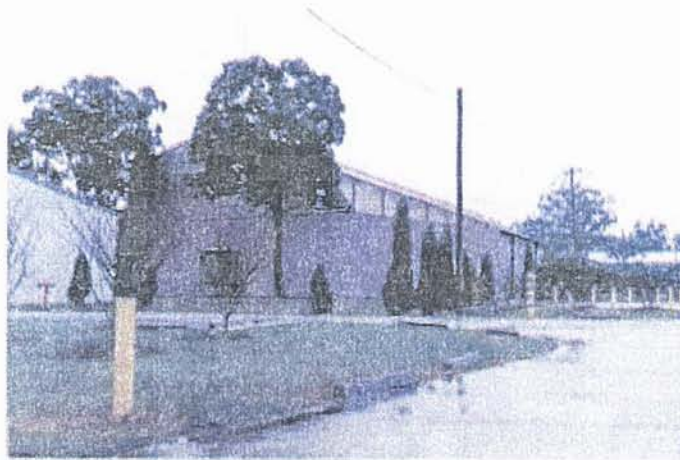
6.0 Existing Environment

6.1 Heritage listed items within the study area

The DNSDC site

The DNSDC site is a rectangular block of land covering approximately 108 hectares. The site is bounded by Anzac Road on the north, Moorebank Avenue on the west, the Greenhills Ave road reserve to the east, and an area of natural bushland on the south and east. The main entrance to the site is located midway along Moorebank Avenue.

Figure 18: Building 9, at the centre of the DNSDC site - a typical WWII composite timber and steel warehouse building (Source: Brooks & Associates 2002:11)



The site includes a number of large storage sheds along with smaller ancillary, administration, and workshop buildings. Among these structures are twenty timber post and beam buildings dating to World War Two. Fifteen of these are of timber post and beam construction, with nine internal bays. They retain their original timber structure, though they have been reclad with modern steel sheeting, and have new concrete floors. Three of the buildings are composite timber and steel warehouses which have three bays of timber post and beam construction on either side of a central raised bay. The central bay has a steel frame to support an overhead gantry crane. The final two buildings are the smaller Quartermaster's Store, with five bays of timber post and beam construction, and the Carpentry Workshop, which are timber framed and three bays wide. Both the Quartermaster's Store and the Carpentry Workshop are constructed of Oregon, an American wood (Brooks and Associates 2002:10).

Figure 19: Interior of Building 79, showing original timber post and beam construction



The alignments of rail sidings that once ran through the site are still visible in the landscape through the location and orientation of some of the buildings and roads, while to the south a remaining siding is still clearly visible.

Figure 20: The visible railway siding to the south, opposite the current Buildings 17 & 18 (previously Buildings 14 & 15)



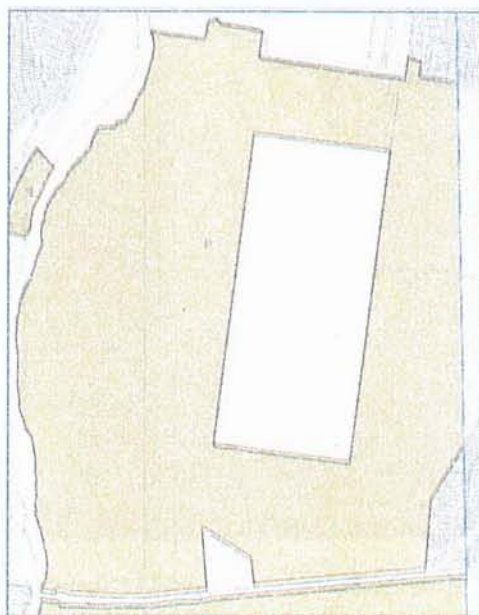
A number of buildings were constructed within the DNSDC site in the mid-late 1990's (Figure 15). These include a cluster of buildings in the south western corner of the site and a number of buildings in the northern section of the site. These buildings do not share the same high heritage values as the WWII structures. Even so, as these buildings are within the DNSDC curtilage as listed on the Commonwealth Heritage Register, the relationship of these buildings to others in the military complex could have some heritage value.

The School of Military Engineering (SME)

Although the section of the SME which is included in the study area is disturbed and does not contain significant heritage items, the significance of the site as a whole should be considered.

The School of Military Engineering is listed on the Liverpool LEP (2008) under its alternate name, the Australian Army Engineers Group (Item 57). This listing notes that the site includes the Royal Australian Engineers (RAE) Memorial Chapel, RAE Monument, Major General Sir Clive Steele Memorial Gates, and The Cust Hut. According to the LEP Heritage map, Item 57 also encompasses most of the land surrounding the DNSDC site, between the East Hills railway line and Anzac Road, as well as a building on the north side of Anzac Road. This building is not specifically mentioned in the LEP, and is listed separately in the State Heritage Inventory as an 'Army Building (Former)'.

Figure 21: Detail of Item 57 on Liverpool LEP Heritage Map (Sheet HER_013)



The main complex of the SME covers approximately 220 hectares between the Georges River and Moorebank Avenue. The SME is accessed from Moorebank Avenue and within its grounds is a group of heritage items associated with the Royal Australian Engineers, including the Royal Australian Engineers monument, the Plant Hangar, and the Memorial Chapel. Located at the south of the site is the Royal Australian Engineers golf course, which overlooks the East Hills rail line. The site is currently in use for army training.

The former army building north of Anzac Road is a long, rectangular corrugated iron shed. This building is some distance from the study area and has no views to or from the study area. Therefore, it will not be impacted by the proposed development.

Figure 22: The locations of features included in Item 57 of the Liverpool LEP (Google Earth)



The rest of the land encompassed by Item 57 on the Liverpool LEP listing now consists mostly of bushland. Since this land was part of Liverpool's military precinct from 1915 and has remained

undeveloped since the 1940s, it is possible that archaeological evidence for military activities survives there. To the north, between the SIMTA site and the residential development at Wattle Grove, is the area used as a rifle range from WWI. Two structures that were visible on an aerial photograph from 1943 are still present at the site.

6.2 Heritage listed items in the vicinity of the study area

There are five locally listed items in the vicinity of the study area. These are Kitchener House, The Holsworthy Group, Casula Powerhouse, and two sets of railway viaducts. There is also one item, Glenfield Farm, listed on the State Heritage Register.

Kitchener House

Kitchener House is listed on the Liverpool LEP (2008) and the Register of the National Estate.

The site is located at 208 Moorebank Avenue, north of the SIMTA site, on an irregular block of approximately half an acre. The single-storeyed Federation-style house is set back about 20 metres from the road, and is surrounded by a landscaped garden which includes a number of mature trees. The house is thought to have been built between 1895 and 1905 and was home to various senior military officers and their families until the 1990s.

Figure 23: Detail of Kitchener House (Item 58) on Liverpool LEP Heritage Map (Sheet HER_013)

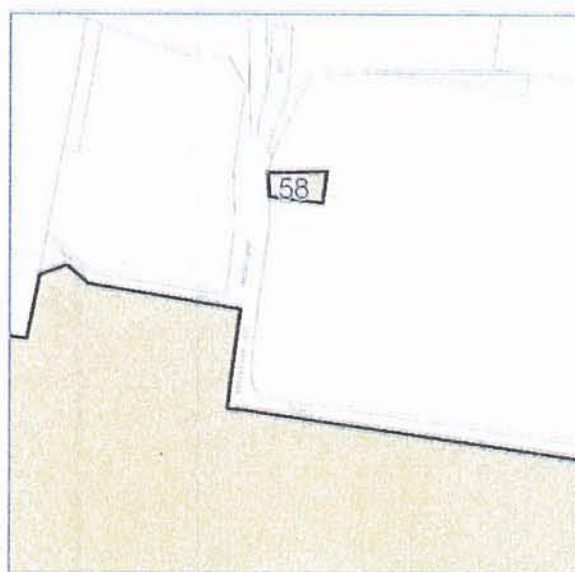


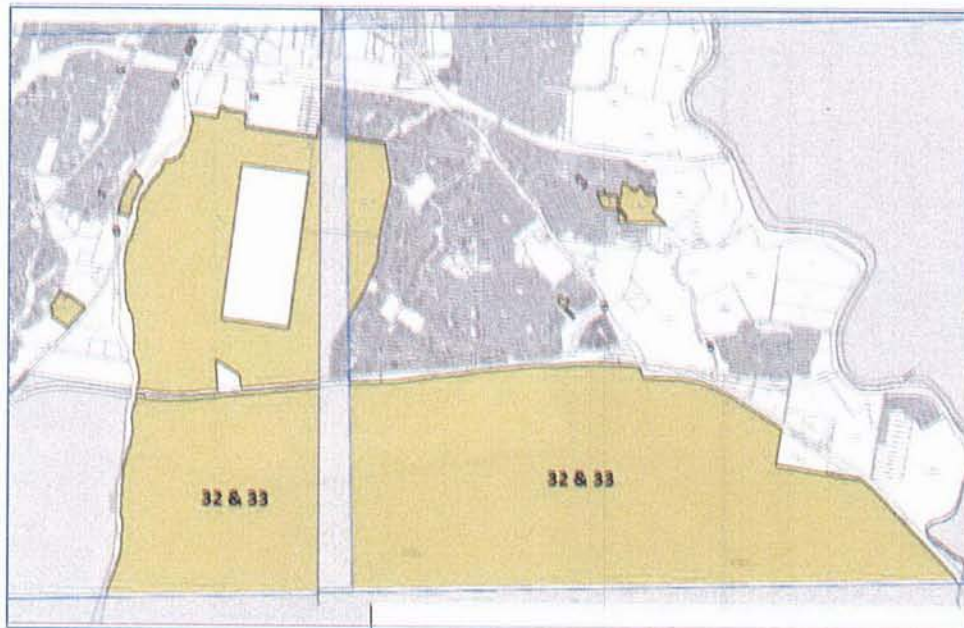
Figure 24: Kitchener House from Moorebank Avenue 2004 (Source: State Heritage Inventory listing "Kitchener House")



The Holsworthy Group

The Holsworthy Group is located within the Holsworthy Training Area, accessed by Artillery Road, Holsworthy. The Group includes the remaining elements of the Old Army Camp and German Concentration Camp, with a collection of early 20th century structures and building remains located around a former parade ground, along with road surfaces and tree plantings.

Figure 25: The Holsworthy Group (Items 32 & 33) as listed on the Liverpool LEP ((Sheet HER_013 & _015)

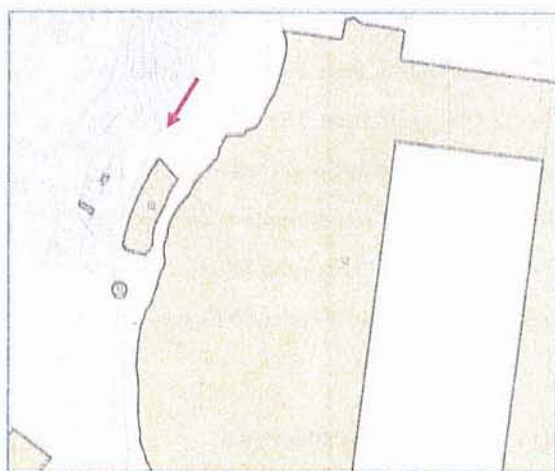


Casula Powerhouse

The Casula Powerhouse is listed on the Liverpool LEP. This item is also known as the Powerhouse Regional Arts Centre and is located to the east of the Casula Railway Station and the Southern Railway line. It consists of the main powerhouse building, which has two adjoining sections of three and four storeys, several ancillary brick buildings, three large steel tanks, and a former coal loading area between the powerhouse and the railway line.

The powerhouse was built in the 1950s by the Electricity Commission of NSW, as one of a number of “package” power stations, all of similar design. These were built to provide interim local generating capacity during a period of power shortage following WWII.

Figure 26(left): Detail of Casula Powerhouse (Item 10) from the Liverpool LEP Heritage map (Sheet HER_013)
Figure 27 (right): Casula Powerhouse from NW (Source: NSW Heritage Database)

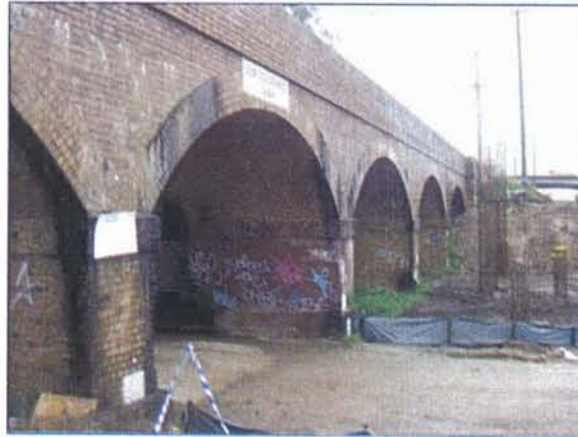


Railway Viaducts

Two sets of railway viaducts are listed on the Liverpool LEP:

Item name	Address	Item number
Railway viaduct	300m south of Casula Powerhouse, Main Southern Railway Line	11
Two railway viaducts	Woodbrook Road, Main Southern Railway Line	12

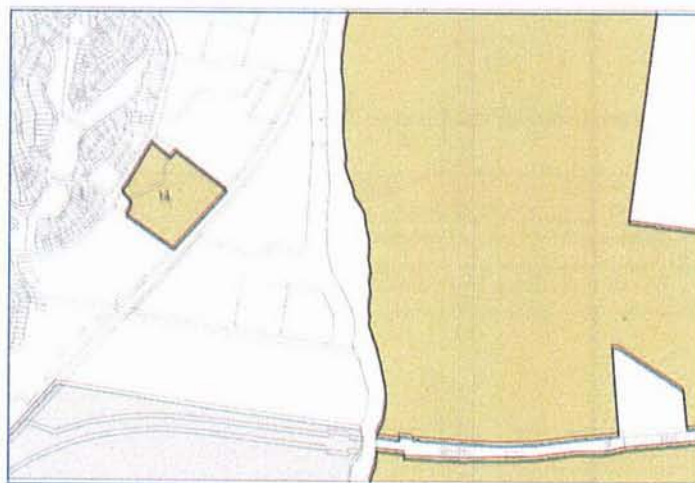
Figure 28: Railway viaduct at Woodbrook Road, Casula (Source: NSW Heritage Office)



Glenfield Farm

Glenfield Farm is listed on the State Heritage Register, and is of exceptional historical significance as one of the few surviving rural farm complexes in New South Wales dating from the original land grant of 1810 and still capable of use for family living and limited farming activities. The buildings on the property are located to the western part of the listed area on top of a ridge and contain a 14 room homestead, a dairy, coach house and privy. The land to the east of the site consists of former rural pastures and the original site fencing (State Heritage Inventory listing "Glenfield Farm"). The curtilage of the item extends down to the Southern Railway Line, and is located only around 50 metres from the south-western extent of the study area.

Figure 29: Detail of Glenfield Farm (Item 14) from Liverpool LEP Heritage map (Sheet_013)



7.0 Impact Assessment

Because designs for the SIMTA project are still being developed, it is not currently possible to prepare adequate Statements of Heritage Impact for the heritage items that will be affected. The following section of this report will assess the significance of each item and provide a preliminary assessment of the potential impact of the SIMTA proposal; however, more detailed impact assessments will be required at the completion of design as part of the Project Application phase/s.

7.1 Heritage listed items within the study area

7.1.1 The DNSDC site

Assessment Criteria

The following assessment of heritage significance of the DNSDC site has been prepared in accordance with the 'Assessing Heritage Significance' (2001) guidelines from the NSW Heritage Manual. The table below outlines a selective summary of the significance assessment detailed in the Australian Heritage Database entry for the site, and the heritage assessment conducted by Brooks and Associates in 2002.

Criteria	Description	Significance Assessment
A – Historical Significance	An item is important in the course or pattern of the local area's cultural or natural history.	The site is highly significant for its association with the development of Australia's military forces since the early 20 th century and particularly for its direct association with the military expansion in the early years of the Second World War. The site has played a continual role in Australia's military infrastructure until the present day.
B – Associative Significance	An item has strong or special associations with the life or works of a person, or group of persons, of importance in the local area's cultural or natural history.	The site has a significant association with the Australian Defence Forces.

Criteria	Description	Significance Assessment
C – Aesthetic Significance	An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in the local area.	The WWII buildings demonstrate the unique aesthetic characteristics of military buildings constructed during the war, and a high degree of technical achievement.
D – Social Significance	An item has strong or special association with a particular community or cultural group in the local area for social, cultural or spiritual reasons.	The site has social significance for the extensive community of Defence personnel who have worked at the site through its history, and for the local community of Liverpool and the broader community of Sydney, as the location of Defence operations since 1915.
E – Research Potential	An item has potential to yield information that will contribute to an understanding of the local area's cultural or natural history.	Moderate scientific significance for its ability to show evidence of the boundaries and alignments of the original land grants in the area, the 1888 Moorebank Farms subdivision, and part of the Liverpool – Anzac Rifle Range – Holsworthy military railway line. Low-moderate archaeological significance for its potential to yield information regarding the early use of the site and, particularly, its use during WWII. The extant WWII buildings have significance for their innovative construction method, as rare and representative examples of timber post and beam store buildings constructed during World War II.
F – Rarity	An item possesses uncommon, rare or endangered aspects of the local area's cultural or natural history.	The group of 18 World War II buildings at the site are the only known surviving group of such buildings in NSW in Defence use. The only other known site with similar World War II timber store buildings, and which remains in Defence ownership, is Bandiana, Victoria.
G – Representative	An item is important in demonstrating the principal characteristics of a class of NSWs (or the local area's): • cultural or natural places; or • cultural or natural environments.	The timber post and beam store buildings have significance as representative examples of this type of store building constructed during World War II for military storage purposes throughout the east coast of Australia.

Statement of Significance

The following Statement of Significance is taken from the Australian Heritage Database entry for the Defence National Storage and Distribution Centre:

“The Defence National Storage and Distribution Centre (DNSDC) is historically highly significant. As a military storage site it dates from 1915, and the Centre is important for its associations with the development of Australia's military forces prior to and during the First World War and particularly for its direct association with the military build-up in the early years of the Second World War. The DNSDC has continued to play an important role in Australia's military infrastructure, right up to the present time. The place also has an association with early nineteenth century settlement in the Liverpool area.

The DNSDC contains twenty Second World War post and beam warehouses, many of which, despite being re-clad, are good examples of their type. Particularly important are the fifteen timber post and beam military warehouses of the nine-bay type which played such an important role during the war and which were the widest post and beam military warehouses. Also important are the three composite steel and timber type warehouses. Post and beam military warehouses are small in number today, giving those at this site substantial rarity value. Additional interest is inherent in the fact that the buildings are understood to have been prefabricated in the United States and shipped to Australia in the early 1940s. Further, the alignment of part of the former military railway system is evidenced by the alignment and siting of some of the buildings and roads at the site.

The Centre is of social value for Defence personnel, for the Liverpool community and for the broader Sydney community on account of the long-term Defence associations with the site.”

Also worth including here is an extract from a 1995 report on Department of Defence Timber Buildings from 1939-1945, which gives a clearer idea of the broader significance of WWII-era standard timber buildings, as part of a nationwide group:

“These buildings are culturally significant as they demonstrate the versatility and self-reliance of the Australian Government and people in a time of national emergency. Under direct military threat, the nation embarked on total mobilisation in its own defence and as it did so, reorganised itself to make the most efficient use of the resources at hand. While the



labour force mobilised and the organisations instigated were largely transient, the facilities constructed and used during the conflict were not. They remain a national asset and a testament to the nation's reaction. That these facilities were often constructed simply, ruggedly and in haste merely demonstrates the extent of emergency. They are culturally significant due to the large public association of these buildings with times of personal and national change and stress.

The buildings are historically significant because the forms and location of the structures depict the strategic reality facing Australia at the time of their construction. Prior to December 1941 development was urgent but considered, and timber construction was largely limited to the scale of building constructed before the war, that is, the timber hut building. These buildings were generally temporary structures for the training of troops. In early 1942, construction in timber was hectic and experimental, concentrating on south eastern Australia. Large complexes were constructed to provide stores, airfields and war industry plant. By 1943, experimentation lessened but the pace of building was maintained. Facilities were developed to bolster Australia's defence and to provide forward supply bases for battles fought in the Pacific Islands. By 1944, the nation served as a storage and staging base for advances throughout the Pacific, and the buildings of that time concentrated around the major posts of Brisbane, Sydney, and to a lesser extent Melbourne.

The buildings have a political and technical significance as they show how standing design preferences and practices were overthrown as part of the national reorganisation. Technologies that had previously had little impact in Australia were used extensively, while technologies introduced by the USA military were embraced. Unseasoned local hardwood, a material that had previously been regarded as unsuitable for large buildings achieved primacy. The technical achievements of this period for timber construction cannot be overstated. The longest span and most widespread timber structures in Australian history were constructed in this period. Almost every species of Australian timber was placed in extreme field test.

This significance has a further facet as the timber construction forms and technologies used throughout the war did not survive it. With demobilisation the pre-war preferences for steel

construction re-emerged and timber construction for structures larger than houses did not recover its national popularity again until the 1960s.

Aesthetically, the truss and other construction forms produced throughout the war are unique. They advanced the aesthetic which had lingered as a legacy from the king and queen posts forms used in Australian buildings with the mortice and tenon construction of the later 1900s, and lack the nostalgia invoked in the 1960s and 1970s for the farmhouse. They express themselves in true and clean engineering layouts as was essential in a time of emergency.

These buildings, located throughout Australia, therefore have a significant heritage value as a group that should be preserved, recognising the influences that determined the form and construction distribution of the group" (Nolan 1995).

Previous Studies

Heritage Assessment – DNSDC site, Moorebank 2002

In 2002, Graham Brooks and Associates (Architects and Heritage Consultants), undertook a heritage assessment for the DNSDC site. This study focused on the built heritage of the site, but did not address its archaeological potential. It was concluded that the site was of significant heritage value as a group of WWII buildings that should be preserved, and recommended that:

- The preferred conservation option for the 18 WWII timber post and beam warehouses is their continued use.
- There should be a detailed feasibility study for the ongoing or adaptive re-use of the timber post and beam warehouse buildings. This should be done either by Defence or a new owner.
- Subject to the re-use study, an acceptable conservation option for the collection of World War II timber post and beam stores buildings could be the retention of one or more of the buildings as a representative example, for continued use by either the Department of Defence, or adaptive re-use by others, provided that a viable re-use of the buildings can be identified.
- Should the re-use of the World War II buildings not be considered prudent or feasible, then demolition of all of the buildings would be acceptable, given the preparation of a photographic recording and measured drawing survey of the site (Brooks and Associates 2002: 27-8).